

McFarland - Stoney Point

-  Livable
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COUNTY

April 13, 2006



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Overview

The McFarland–Stoney Point LCI Study Area is the culmination of a detailed planning effort with a public outreach component involving stakeholders from Forsyth County. This overview introduces the Livable Centers Initiative, Study Area location, Study Area goals, historical context, and a preface to this document.

Livable Centers Initiative

This study was funded by the Livable Centers Initiative (LCI) program directed by the Atlanta Regional Commission (ARC). This study has been conducted in accordance with ARC guidelines, yet has been customized to serve the unique nature of the Study Area as an emerging activity center.

The ARC has adopted a regional plan whereby future development is concentrated in activity centers and along corridors. The overarching vision is to create livable communities which are not dependent on the automobile.



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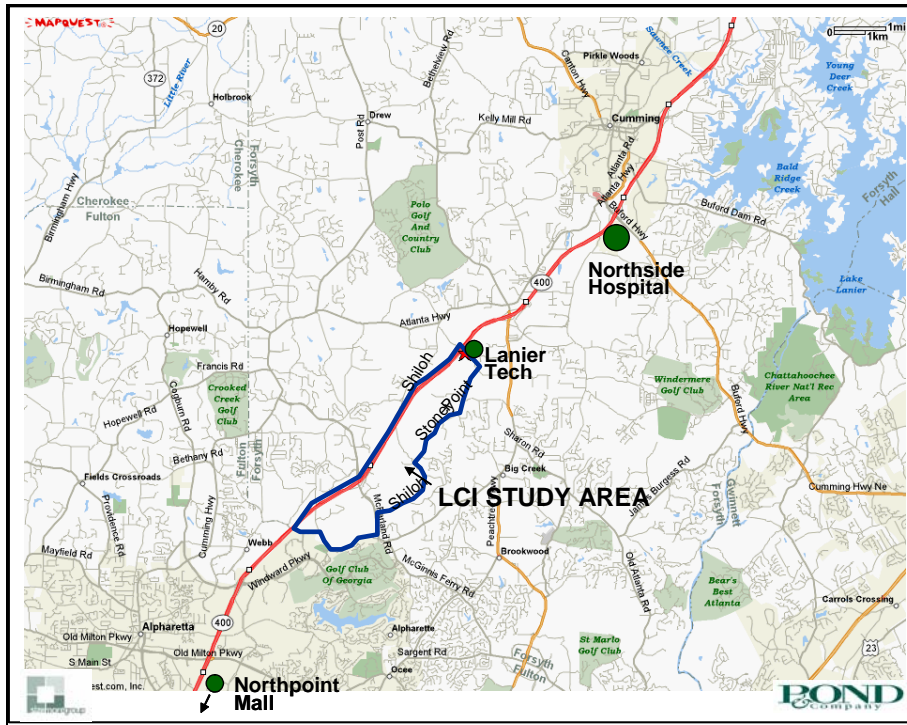
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Study Area Location

The McFarland-Union Hill LCI Study Area is located in the southwestern corner of Forsyth County immediately north of the Fulton County line and southeast of the Cherokee County border. As illustrated in the map below, there are several key points of interest within close proximity of the Study Area including Northside Hospital, Northpoint Mall, Lanier Tech and Lake Lanier.

Extending approximately 4.5 miles northeast, the Study Area is bounded by GA 400 on the west, McGinnis Ferry Road to the south, Shiloh Road and Stoney Point Road to the east, and Majors Road to the north. The Study Area size is approximately 2,150 acres.



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Study Area Goals

The ARC's goals for Livable Centers, such as the McFarland–Stoney Point LCI, include:

- Connect homes, shops, & offices ;
- Enhance streetscapes & sidewalks ;
- Emphasize the pedestrian ;
- Improve access to transit & other transportation options ; and,
- Expand housing choice.

The Forsyth County Comprehensive Plan states similar goals for the Study Area, including:

- Provide connectivity between and within uses ;
- Encourage alternative modes of transportation ;
- Establish activity centers at major intersections ;
- Discourage “strip development” patterns ;
- Encourage mixed-use developments ;
- Establish architectural and design standards ; and,
- Connect activity centers to adjoining residential neighborhoods with sidewalks.

The Community's vision for the McFarland Subarea (which nearly encompasses the Study Area) is outlined in the McFarland Subarea report and has four dimensions:

- The McFarland subarea will be the premier employment center for the County.
- It will be the site of quality development and business opportunities.
- Convenient access to commercial services will be located along major arterial roads.
- A network of greenways will run along the Big Creek with passive parks and connected trails.

Specific policies for the McFarland Subarea recommended in the SubArea Report include:

- Prepare a Greenway Master Plan connecting greenways throughout the County ;
- Provide more public parks ;
- Create hiking trails along Big Creek that connect with North Fulton County ;
- Control the amount of multi-family residential allowed (with the area north of Windward Pkwy and east of GA 400 as an example of the appropriate mixture of residential) ;
- Create design standards for McFarland Road ;

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- Create incentives for constructing mixed-use developments ;
- Create more grid pattern streets in large developments ;
- Improve transportation facilities to serve the Future Land Use Plan ;
- Improve public safety, library, and postal services.

Fortunately, the ARC, County, and Subarea goals are generally in accord. The ultimate purpose of this study is to create a plan to best meet these goals as well as those newly identified by the local community during the study process.

Historical Context

Forsyth County was originally Cherokee Nation East, held by the Cherokee Indians. In 1829, gold was discovered nearby, in what is now known as Dahlonega. By 1832, the Cherokees had been removed from the land and it was purchased by settlers through land lotteries. The County was named for John Forsyth, a former Governor and Secretary of State. The City of Cumming was established two years later. Forsyth County's history is largely shaped by new people coming in to settle in the community. The gold rush originally brought people to the area; later it was the poultry industry. The creation of Buford Dam has also helped to attract people to the area.

Forsyth County was primarily rural land for much of the 20th Century. There were fully functioning farms throughout the County. There was a large population of horse farmers, and those that owned large country estates. As metro Atlanta grew, Forsyth County has changed. Until the mid-1980s, this area was still considered a rural segment of the Atlanta region. By the 1990s, as GA 400 was extended and more and more development moved northward from the Perimeter area, the area really began to change.

The direction of growth was cemented for the area when North Point Mall was built in Alpharetta. It made Alpharetta, just to the south of the Study Area, a viable suburban community. As the residential base grew, other commercial uses were drawn to the area. Office also began to grow in and around the area as well, and provided the impetus for what is today Windward Parkway.

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The natural path of development would have made a logical trek towards the Study Area, in a northward route from Windward Parkway. But, the growth was faster than anyone realistically expected. At the time, Forsyth County did not have adequate water and sewer capacity, and had to purchase capacity from Fulton County. Because of this, and other infrastructure issues, development basically “skipped over” the Study Area and headed further north. Retail and commercial development went further up to Cumming, and eventually created a destination in Dawsonville with high-end outlet shopping. To the south, office development became more concentrated in the Alpharetta area as a result.

Office and industrial uses began to develop in the Study Area very slowly. In the late 1990s and the early 2000s, the area was dominated by industrial uses, taking the shape of business parks and office campuses primarily. The line was blurry for office and industrial uses, as many co-located in business parks because of cheaper rents, in comparison to the North Fulton market.

Residential development was basically non-existent in the Study Area for much of this time, except for isolated estates and farms. In the early 2000s, residential interest took firm hold in the area. The lack of amenities and access to goods and services in the area is part of the reason for the slow growth patterns. Once the development “skipped over” the Study Area, it has basically become almost a “donut hole” between the Alpharetta/Windward Parkway area and the Cumming area.

Preface

This document was developed to communicate the findings and recommendations for the McFarland–Stoney Point LCI Study Area and is divided into the following sections:

Section A – Community Assessment

The Community Assessment offers a complete documentation of the base condition of the Study Area by various planning elements and includes: existing conditions analysis; land use and zoning; recent development activity; transportation and circulation; demographics, socio-economics and employment; and market analysis.

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Section B – Development Plan

The Development Plan outlines the vision established by the Forsyth community through the public outreach process, including future land uses and locations, a circulation plan, and potential development opportunities. Included in this section are area-specific design concepts and projects, high-priority projects and proposed transportation improvements. This section includes the Community Vision (results of the Community Preference Survey), LCI Study Area Framework and Development Opportunities including information and data on proposed LCI development/improvement projects.

Section C – Synthesis and Action Plan

The Action Plan describes mechanisms for implementing the land use and transportation recommendations of the Development Plan. Elements of this section include implementation mechanisms; recommendations such as an organizational framework and necessary initiatives to achieve it, future land use classification changes, and an urban design overlay district; identifies Potential Underlying Zoning Districts; provides a 5-Year Strategic Plan (projects, costs, funding) and a 25-Year projection of population and employment within the Study Area; and descriptions of how the Plan addresses LCI program goals.

Appendices

Three appendices offer definitions and resources, detailed market and planning information, and documentation of the public involvement component of the planning process.

Note that an Executive Summary was also prepared as a supplement to this document in order to concisely report key findings and recommendations of the FORWARD FORSYTH planning process.



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Acknowledgements

This document represents six months of work by a Planning Team consisting of Forsyth County representatives and a consulting team lead by Pond & Company. The Planning Team wishes to thank all who participated in the development of this plan for their time, leadership, and vision. The team is particularly grateful for the contributions of the following participants in the planning process.

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Section A. Community Assessment

Existing Conditions Analysis

The Analysis documented in this section describes important data and assumptions that are accepted as “givens” for this project. This section includes the following topics:

1. Base Conditions
 - Land Cover (aerial)
2. Natural Features
 - Topography
 - Creek Beds and Floodplains
3. Physical Analysis and Urban Design Features:
 - Gateways and Zones
 - Building Condition
 - Housing Clusters
 - Critical Edges and Views
 - Unique / Significant Facilities
 - Walking Radii and Scale



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Base Conditions – Land Cover:

Recent aerial imagery introduces to the baseline condition of the McFarland–Stoney Point LCI Study Area. Though the Study Area is in a state of change with numerous projects under construction, the following basemap (map A-1) offers a snapshot of the area. It is clear from the image that the area is in a primarily undeveloped state. The swath of tree cover and agricultural fields is interrupted by industrial scale facilities at select locations along existing roadways. Concentrating development in order to preserve the rural character and open spaces of the Study Area is a community priority.



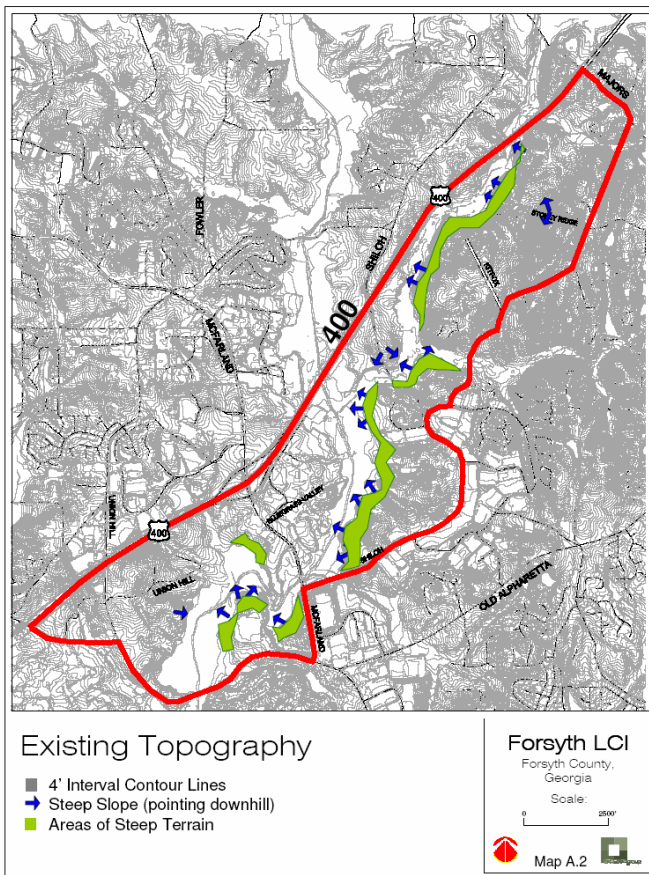
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Natural Features – Topography:

As is the case with most of metro-Atlanta, the Study Area may be categorized as having rolling topography with little to no flat lands outside of the floodplains. The accompanying photographs portray the present landscape. As illustrated in map A-2, there are several zones within the Study Area with severe grades that may be a constraint for future development. The light green areas identify places where the terrain is so steep that development is typically less cost effective because construction is more difficult compared to sites of more gentle terrain.



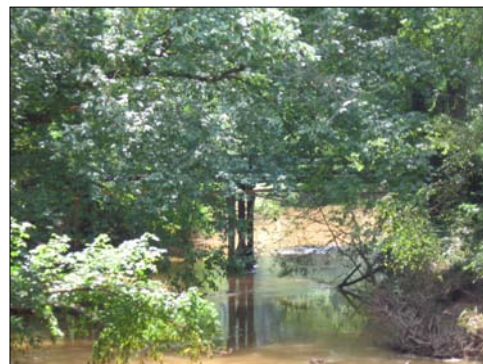
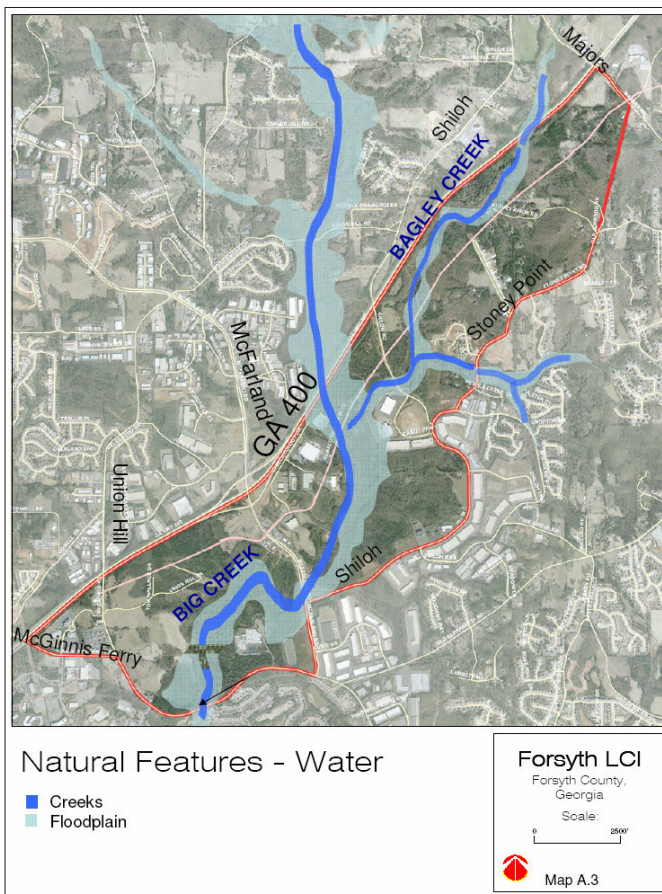
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Natural Features – Creek Beds and Floodplains:

Creek beds and floodplains (mapped below) are prime locations for preservation as greenspace because they host native wildlife and support abundant vegetation. Much of the Big Creek floodplain is identified as “swamp/marsh” in the McFarland Subarea report and is not suitable for development. The Future Land Use Plan denotes floodplains and buffers along creeks as “Conservation” lands.



Floodplains and creeks should not be formally developed, but residents have expressed strong support for multi-use trails along greenway corridors as not only a recreational amenity but also a critical part of a greater bike/ped network.

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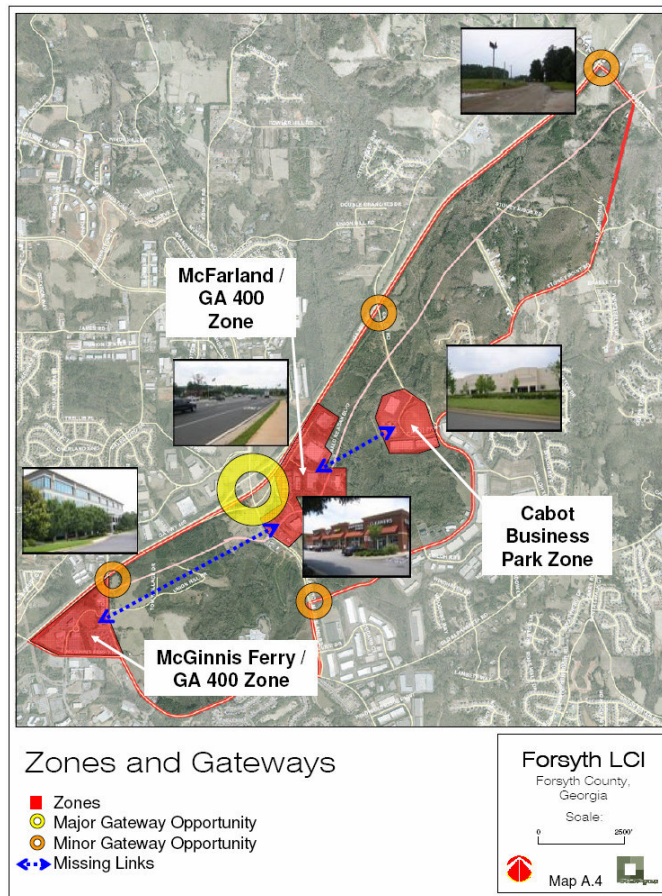
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Urban Design Features

Gateways and Zones:

Several locations serve as key entry points to the area and may be considered for gateway landmarks to let travelers know they have arrived at a unique destination. Though the rural, ranch character of the area is obvious, the built environment lacks an easily identifiable sense of architectural character or identity. Still, there are currently three distinctive clusters, or zones, of existing non-residential development within the Study Area. No local streets directly link these. The following map identifies gateway locations, zones, and missing links.



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The first zone is the McFarland / GA 400 Node which has relatively newly constructed one-story commercial buildings which front McFarland Road. The northeast portion of this node, along Technology Drive, has a distinctly different character made up of large freestanding office/industrial buildings on individual, non-connected parcels.



Large office/industrial at McFarland/GA 400 Node



Retail at McFarland/GA 400 Node

The second distinct zone is located at the McGinnis Ferry / GA 400 Node and is comprised primarily of large and small scale office parks. The architectural styles at this node range from mid-rise modern contemporary, to one- and two-story Williamsburg traditional. Within this node there is also a fair amount of undeveloped land slated for future office development.



Williamsburg-style office park at McGinnis Ferry/GA 400 Node



Newly constructed modern office park at park at McGinnis Ferry/GA 400 Node



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The third zone is the Cabot Business Park which is located along Cabot Parkway, off of Shiloh Road, at about the midpoint of the Study Area. The character here is that of large, big-box office warehouse buildings, two of which are currently vacant. These massive buildings do not address the street or the pedestrian environment, are not interconnected, and do not have shared parking.



Large scale vacant office/warehouse building within the Cabot Business Park



Vacant Publix warehouse/distribution building - Cabot Business Park

Building Condition

The development that exists is relatively new and of high building quality; most properties are in very good condition. Based on a field survey, there were very few properties in the Study Area that had visible signs of deferred maintenance, were unoccupied, or in need of obvious repair.



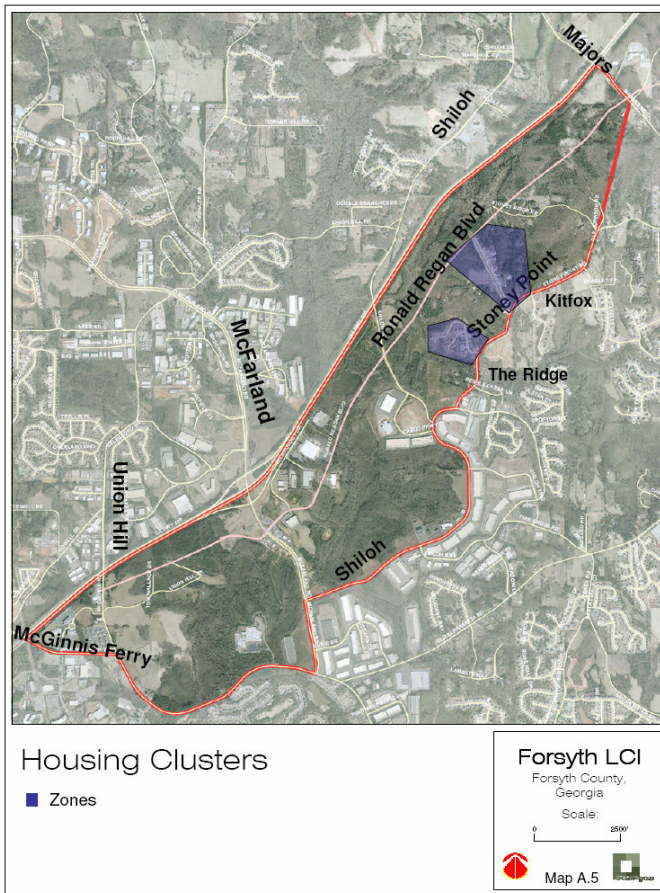
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Housing Clusters

Interspersed between and around the non-residential zones is a mix of single-family and estate homes in both freestanding and subdivision developments. While these developments are currently not linked by internal connections, it will be increasingly important to encourage interconnectivity as the area develops to help alleviate traffic on the arterial and collector roads in the area and to provide a multitude of transportation options for getting from point A to point B. The following map identifies two existing, identifiable neighborhoods.



Example of residential style in the area



Relatively higher density residential in the area



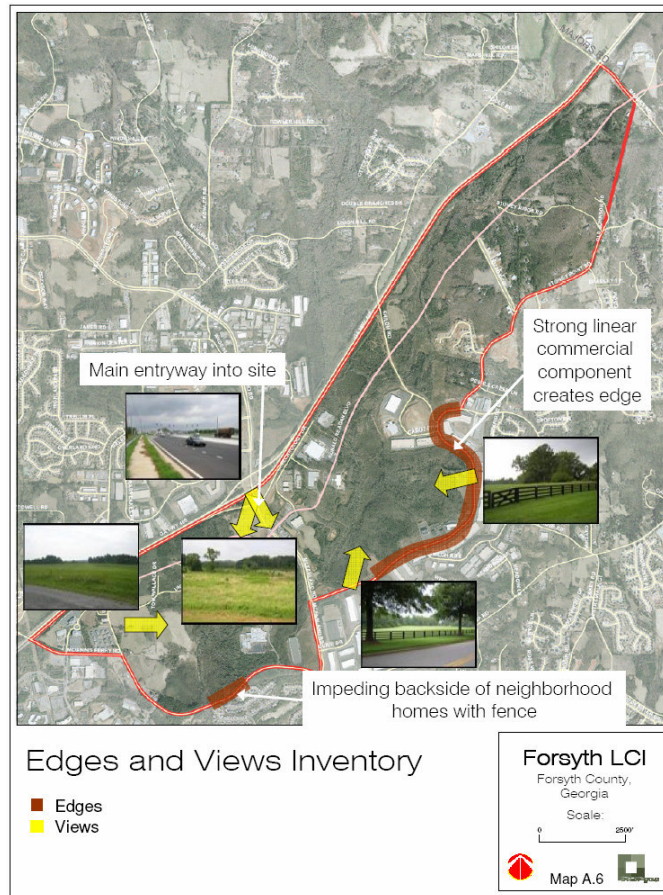
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Critical Edges and Views

It is important to be mindful of the existing context when considering future development. As illustrated in map A-6, there are certain edges and views which may be preserved or enhanced along major roads such as Shiloh Road, McGinnis Ferry Road, and McFarland Road. These help define the Study Area and orient the visitor.



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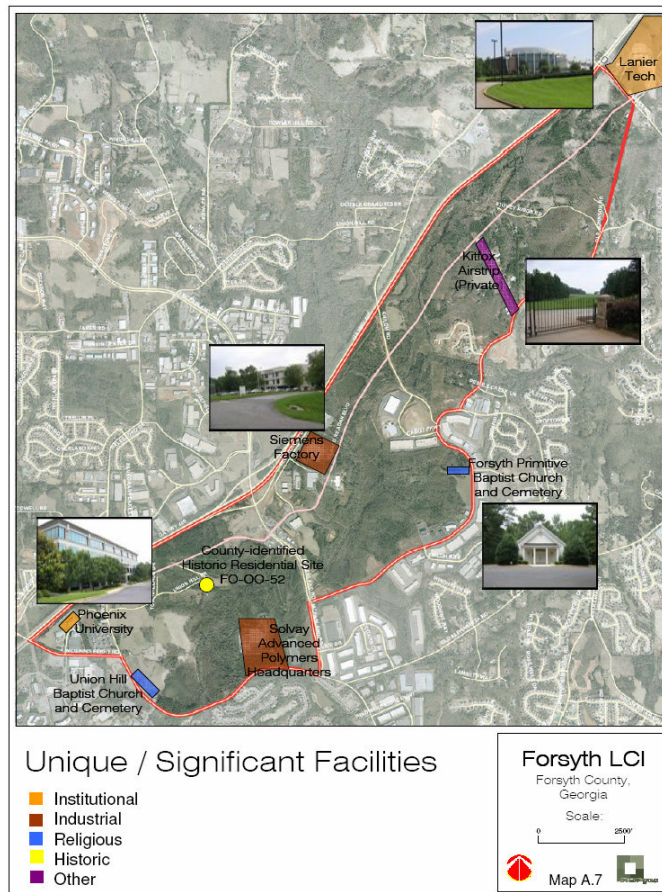


Unique / Significant Facilities

Map A-7 locates a variety of unique and significant developments in the Study Area include:

- Forsyth Primitive Baptist Church
- Union Hill Baptist Church and Cemetery
- Kitfox Airstrip
- Lanier Tech
- Siemens Factory
- Solvay Advanced Polymers Headquarters

These facilities are resources which new development plans should respect and enhance. Provisions should be made for connections to Lanier Tech.



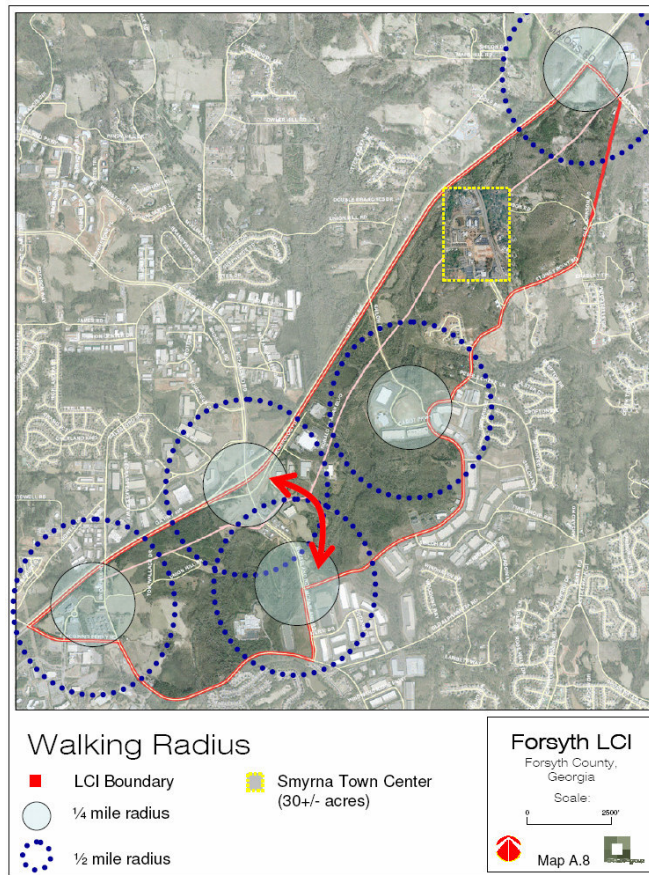
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Walk Radii and Scale

Due to the large size of the Study Area, it is important to be mindful of 5- and 10-minute walking radii, estimated as quarter and half mile distances, which most people are willing to walk, within a pedestrian scale environment. The following map illustrates the reality of the Study Area size (notice the relative scale of the Smyrna Town Center). Besides recreational or bicycle trips, no pedestrian should be expected to regularly travel by foot even halfway across the Study Area. This affirms the need to locate compact, mixed-use development in walk-able centers.



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Existing Land Use and Zoning

The following is an assessment of Existing Land Use and current Land Use Regulations in the area, including: the Comprehensive Plan for Forsyth County (the source of the Future Land Use Map and Categories as well as Goals for the County and McFarland Subarea) and the County Zoning Ordinance (Map and Districts).

Existing Land Use

The Study Area is primarily in an undeveloped state. The industrial, commercial and residential development that does exist is of a very low density consisting mainly of one- or two-storied buildings with surface parking lots accessory to the buildings. Uses are connected by sidewalks only along McFarland Road near GA 400, and internal sidewalks exist only within residential subdivisions. The number of streets and thoroughfares are not enough to support higher density or mixed-use development in their current forms.

As of 2004, the Study Area's existing land uses (excluding right-of-way) are broken down as follows:

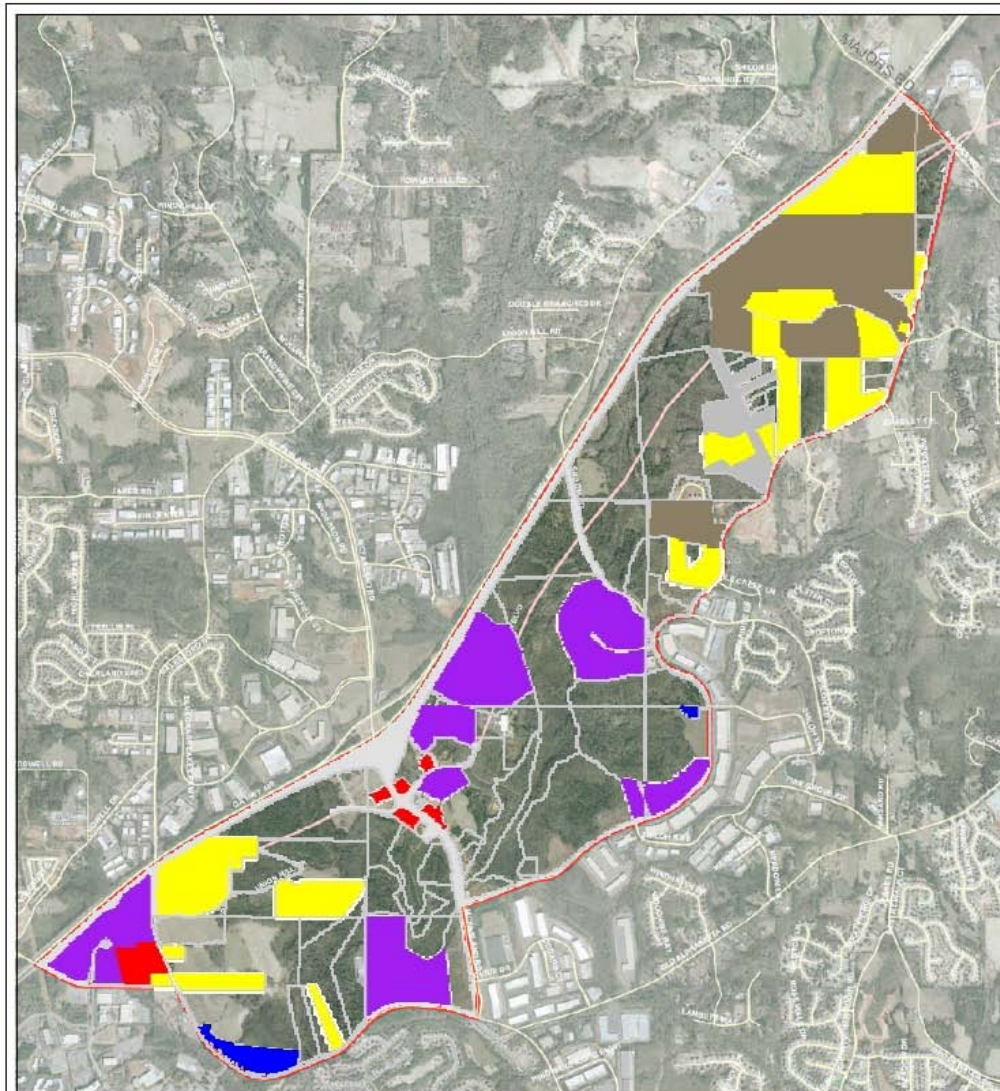
LAND USE CLASSIFICATION	% OF STUDY AREA
Undeveloped	60 %
Residential	13 %
Industrial	12 %
Agriculture	11 %
Transportation/Com/Utilities	2 %
Commercial	1 %
Institutional/Public	1 %

Some of the parcels identified as "Residential" use are single homes on very large lots; some of these have already applied for redevelopment.

Note that additional detail regarding the existing development in the area is provided in the Market Analysis section. The following is a map of existing land use in the Study Area as of 2004.

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Existing Land Use Map 1 of 1
Existing Land Use (2004)

- | | |
|--|--|
|  Residential |  Agriculture |
|  Commercial |  Right of Way |
|  Industrial |  Undeveloped |
|  Institutional/Public | |
|  Transportation/Com/Utilities | |

McFarland – Union Hill LCI
Forsyth County, Georgia
Scale: 0 2500'




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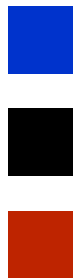
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Future Land Use

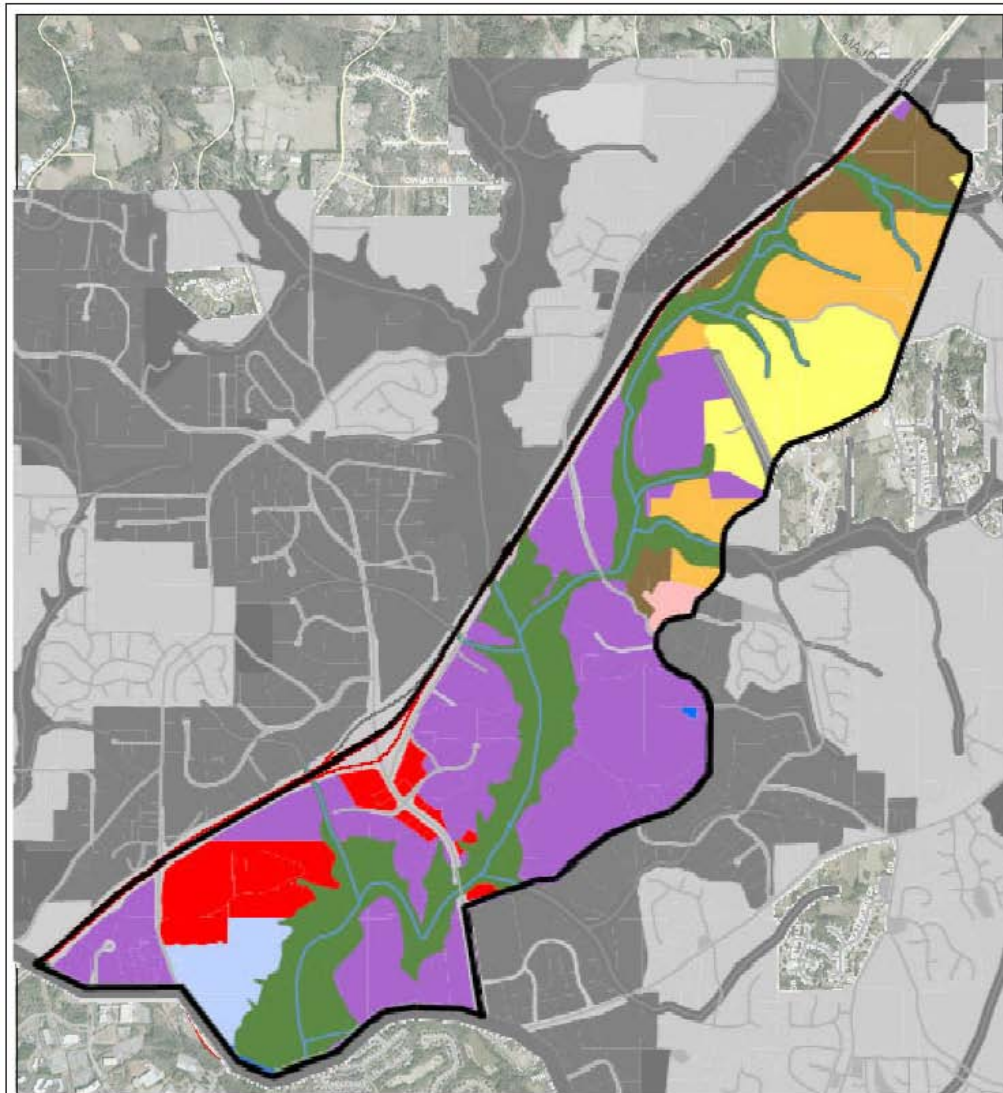
The future land use classifications of the Study Area are predominantly industrial and conservation, which accurately reflect the zoning and land use designations present today. The remainder of the Study Area is made up of residential and commercial land use designations which constitute a minority of the area.

LAND USE CLASSIFICATION	% OF STUDY AREA
Conservation	17%
Public/Institutional	1%
Low Density Residential (~ 1.5 units per acre)	9%
Medium Density Residential (1.5 ~ 2.5 units per acre)	9%
High Density Residential (2.5 ~ 6 units per acre)	8%
Industrial	41%
Neighborhood Commercial	3%
General Commercial	8%
Activity Center	4%



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Future Land Use Map 1 of 2
Future Land Use Plan (Current)

- | | | |
|-----------------------|---|--|
| — Road | Low Density Residential (~ 1.5 units per acre) | Activity Center |
| — Major Road | Medium Density Residential (1.5 - 2.5 units per acre) | Public/Institutional |
| — Georgia 400 Highway | High Density Residential (2.5 - 5 units per acre) | Public Park |
| — River/Stream | General Commercial | Private Park |
| — Lake/Lanier | Comdor Transitional | Transportation/Communication/Utilities |
| — City Boundary | Neighborhood Commercial | Metropolitan River Protection Act 2000-foot Corridor |
| — County Boundary | Industrial | |
| — Conservation | Landfill | |

McFarland – Union Hill LCI

Forsyth County, Georgia

Scale: 0 2500'



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The land use classifications prescribed are defined in the Comprehensive Plan as follows:

Low Density Residential. This category is intended for single-family detached residential dwellings typically on septic sewer systems. Minimum lot size is 30,000 square feet; overall density is up to 1.5 units per acre. Institutional uses that are considered complimentary in this area include churches, schools, and libraries.

Medium Density Residential. Consists of single-family detached residential dwellings typically on sewer, with overall densities from 1.5 to 2.5 units per acre. Institutional uses that are considered complimentary in this area include churches, schools, and libraries. Private recreational uses, such as golf courses, tennis courts, walking trails and swimming pools are encouraged in these areas in developments greater than 50 lots.

High Density Residential. Typical uses include single-family detached and attached dwellings (such as townhouses) as well as multifamily condominiums and apartments, at densities of up to 6 units per acre. These developments should incorporate some amount of central outdoor public space for their residents. A limited number of churches, schools, libraries, other miscellaneous institutional uses and daycare facilities would be complimentary in these areas. An urban-density residential area is usually located near areas of high employment concentration, large commercial nodes, transportation nodes and mixed-use developments. It is essential that these developments include proper buffering between adjacent land uses.

Neighborhood Commercial. This category includes a limited range of retail and service activities to serve the everyday needs of local residents. Limitations should apply to both size and character of individual establishments. The basic character of this category is one that encourages and assures a compatible mixture of residential, office and retail types of land uses. Businesses in this land use category should be designed to encourage the development of neighborhood scale shopping that offers both goods and products, and the furnishing of selected services.

General Commercial. This category includes standard retail and commercial service activities such as shopping centers, general retailers, specialty shops, grocery stores, drug stores, banks, restaurants, theatres, hotels, dry cleaners and entertainment facilities. These establishments should

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be located on appropriate transportation corridors to easily serve the public. Visual impacts of these developments should be minimized by requiring buffers, landscaping, and architectural controls.

Activity Center. This category includes commercial, entertainment, limited residential, and public/institutional land uses typically found in a central business district. The commercial business district (CBD), neighborhood shopping (NS), master planned district (MPD), and urban village (UV) zoning districts are appropriate for developments proposed within an Activity Center, though developments may be encouraged to consider additional zoning classifications upon future revisions to the Unified Development Code. The types of uses that are desirable in this area would be restaurants, specialty retail, governmental offices, low-intensity offices (e.g. accountant or real estate office) and appropriate parking. Uses should be complemented with walking, biking and transit opportunities to provide alternative modes of transportation. Also, any roadway improvements that are considered for this area should carefully consider the scale of the area. Residential uses shall be considered accessory and limited to townhouses or apartments mixed into the commercial establishments. Commercial uses must be developed or present first in comparable scale before residential uses can be permitted for construction.

Industrial. These areas are where light industrial uses can be located; such uses include light manufacturing, warehousing, wholesale/distribution and assembly. More intensive industrial uses that are characterized by noise, fumes, vibration and other forms of pollution that might be viewed as objectionable by surrounding uses are acceptable only on a case-by-case basis if well screened and buffered from other uses. Limited commercial uses are also allowed in industrial areas if they compliment existing industrial uses, such as cafeterias and day cares.

Public / Institutional. Includes uses such as schools, colleges, hospitals, municipal community centers, churches, cemeteries, municipal buildings and post offices.

Conservation. This classification includes environmentally sensitive areas that are regulated at the local and state levels. Areas include a 50-foot buffer along all County streams and land in the 100-year floodplain of local waterways. This land has the potential to be used as public greenways or multi-purpose paths. The Conservation classification alerts government officials and property owners



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to the presence of environmental conditions which should to be factored into the rezoning and permitting process.

Comprehensive Plan Land Use Goals

In 2003, the County updated the Forsyth County Comprehensive Plan through an ambitious public involvement process. In order to better analyze the County's needs and elicit a higher degree of community participation, the county was divided into ten planning sub-areas, including the McFarland Subarea. Subarea boundaries were determined by a variety of factors including: physical barriers, historical communities, economics and transportation corridors.

Information was gathered to enable the community to look at where they stood and how they wanted to develop. The study's findings presaged some of the goals and recommendations of the current study. Concerning the vision for this area, it was hoped that the McFarland Subarea would be a premier employment center for the County, an area of quality development and business opportunities and have a network of greenways running along the Big Creek with passive parks and connected trails.

Regarding land use, it was thought that design standards would be needed, land for public parks would be necessary, incentives for constructing mixed-use developments should be provided, a grid pattern of streets in large developments should take place, and transportation improvements would be needed to serve the Future Land Use Plan.

Among the land use recommendations was the creation of a Neighborhood Commercial category which included a limited range of retail and service activities to serve the everyday needs of local residents. Limitations would apply to both size and character of individual establishments. The basic character of this category is one that encourages and assures a compatible mixture of residential, office and retail types of land uses. Businesses in this land use category should be designed to encourage the development of neighborhood scale shopping that offers goods and products and the furnishing of selected services.

An Activity Center land use category was also suggested; this would include commercial, entertainment, limited residential, and public/institutional land uses typically found in a central

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business district. The commercial business district (CBD), neighborhood shopping (NS), master planned district (MPD), and urban village (UV) zoning districts are considered appropriate for developments proposed within the Activity Center classification. Developments within the Activity Center classification may be encouraged to consider additional zoning districts, based on future revisions to the Forsyth County Unified Development Code.

The types of uses that are desirable in this area would be restaurants, specialty retail, governmental offices, low-intensity offices (e.g. accountant or real estate office), and appropriate parking. Uses should be complemented with walking, biking, and transit opportunities to provide alternative modes of transportation. Also, any roadway improvements that are considered for this area should carefully consider the scale of the area. Residential uses shall be considered accessory and limited to townhouses or apartments mixed into the commercial establishments. Commercial uses must be developed or present first in comparable scale before residential uses can be permitted for construction.

Lastly, a Corridor Commercial category was intended to focus on major transportation corridors, which presently contain a mix of agricultural, residential, commercial, and industrial land uses. Such corridors are unlikely to experience small scale, low or medium density residential development over the course of the planning period.

Developments within the corridor commercial classification will focus on land uses of varying intensity that will allow for the appropriate transition between high-intensity development abutting the transportation corridor as well as provide for a continued decrease of intensity as the development moves away from the transportation corridor. The physical character and design of proposed developments should be compatible with surrounding uses. Inter-parcel connectivity and side street access should be encouraged to minimize curb cuts and improve traffic flow.

The existing character of Study Area in addition to the existing zoning of the Study Area stands in direct contrast to the Land Use Community Goals as defined by the 2004 Comprehensive Plan for Forsyth County. This Plan establishes clear goals for future development that the LCI Study Area currently does not meet. This LCI Plan is an opportunity to meet the following Community Goals as defined in the Comprehensive Plan:

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Community Goal 1. Encourage responsible development patterns and policies that maximize the resources of both the public and private sector by:

- Providing connectivity between and within uses
- Encouraging alternative modes of transportation, establishing activity centers at major intersections, and discouraging “strip development” patterns
- Ensuring development provides the installation of sidewalks, bike paths and street trees

Community Goal 2. Improve opportunities for citizen involvement in the development process by:

- Providing an efficient and adequate level of public participation in the study process

Community Goal 3. Provide an efficient, equitable and compatible land use plan that combines the community's desire for a high quality of life with respect for private property rights by:

- Update the Unified Development Code to reflect the recommendations of the LCI Plan

Community Goal 4. Encourage creative and flexible site design to accommodate site-specific conditions by:

- Encouraging mixed-use developments
- Establishing architectural and design standards
- Reducing urban heat islands and minimizing light pollution
- Providing street designs that improve traffic flow and reduce congestion, such as grid pattern streets

Community Goal 5. Protect the natural, historic, and scenic qualities of the county by:

- Preparing a greenway/open space master plan
- Preserving greenbelt protected areas, such as the Big Creek
- Preserving trees and vegetative cover
- Recognizing the need for passive recreational spaces
- Developing strategies for improving watershed quality
- Encouraging maximizing open space with density incentives
- Requiring mixed-use development to provide greenspace
- Providing appropriate greenspace buffers around commercial developments
- Connecting activity centers to adjoining residential neighborhoods with sidewalks

McFarland - Stoney Point Livable Center Initiative

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Community Goal 6. Encourage the development of a sustainable tax base through an increase in business/commercial land use by:

- Achieving an approximate 60% residential and 40% nonresidential mix of uses
- Encouraging adequate workforce housing opportunities

Community Goal 7. Ensure consistency of residential development with the subareas' visions and balance the needs of landowners and residents by:

- Protecting residential areas from negative impact by incompatible adjoining uses or adverse environmental conditions
- Factoring in the usability of land when determining appropriate densities
- Providing residential densities and designs that ensure a variety of housing types

Community Goal 8. Develop overlay districts:

- Develop overlay districts for implementation of the LCI Plan which will include design criteria, alternative development regulations, and increased flexibility in accepted land uses.



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Existing Zoning

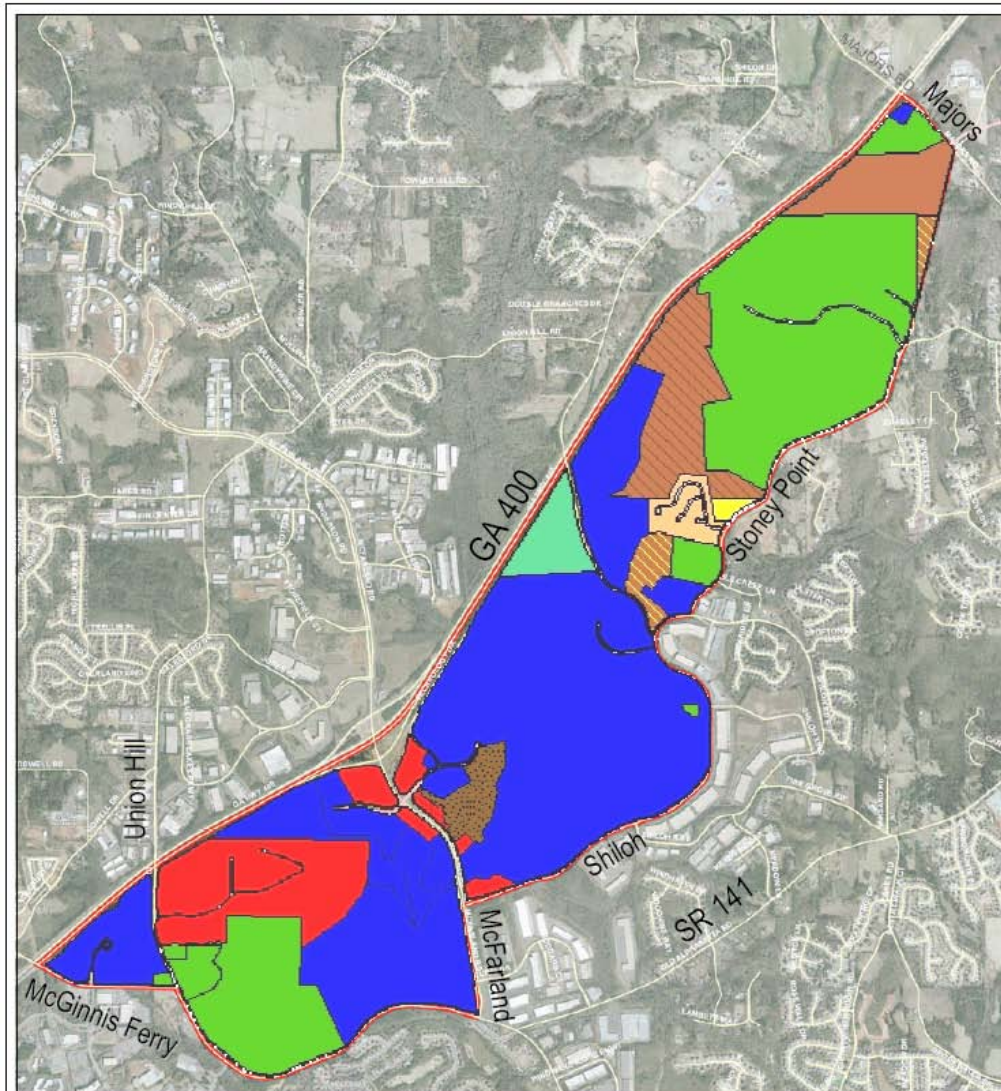
Within the Study Area itself are two distinct halves: an industrial subarea and a residential subarea. The industrial subarea is in the southern part of the Study Area bound by GA 400, Union Hill Road and McGinnis Ferry Road which contains primarily industrial land uses and zoning designations. The remaining residential subarea is bound by GA 400, McGinnis Ferry Road and McFarland Road and is predominantly residential land uses and zoning designations. Forsyth County's Big Creek runs through the Study Area as well and this area is designated as a Conservation Land Use classification throughout the Study Area. The majority of properties are vacant and not developed and those that are developed are generally one-story industrial structures or large to mid-size lot residential developments.

Forsyth County does have three progressive zoning districts that are located in close proximity to the Study Area that match the goals of the LCI program (Master Planned, Neighborhood Shopping, and Urban Village), however none of these districts are located within the Study Area itself. However, these are only located on isolated properties and make up a tiny fraction of the entire area. These districts include provisions for square footage limitations, connectivity and roadway design, building height and mass, arrangement and siting of buildings, building appearance and architectural details, signage, parking, landscaping, public spaces and streetscapes, pedestrian and bicycle access, transit access and lighting.

ZONING DISTRICT	% OF STUDY AREA
A1 – Agricultural District	22%
AG-RES – Agricultural Residential District	5%
R1 – Single-family Residential District	1%
R2 – Single-family Residential District	2%
RES 3 – Single-family Residential District (3 units per acre)	2%
RES 4 – Single-family Residential District (4 units per acre)	10%
RES 6 – Single-family Residential District (6 units per acre)	4%
M1 – Restricted Industrial District	46%
CBD – Commercial Business District	8%

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Current Zoning Map 1 of 2

Existing Zoning Districts

- | | |
|--|---|
|  R1 Single Family Residential |  A1 Agricultural |
|  R2 Single Family Residential |  AG-RES Agricultural Residential |
|  RES3 SF Residential three units per acre |  CBD Commercial Business |
|  RES4 SF Residential four units per acre |  M1 Restricted Industrial |
|  RES6 SF Residential six units per acre | |

McFarland - Union Hill LCI

Forsyth County, Georgia

Scale: 0 2500'



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Recent Development Activity

The bulk of the Study Area that is presently developed is in industrial and office uses. Residential development started in earnest in the early part of this decade and has intensified this year. There is currently no significant retail located in the Study Area. But, all of that is about to change. The following is a review of recent development activity in the Study Area; note that additional detail regarding development trends is provided in the Market Analysis section.

There is an incredible amount of development under construction, planned, and approved for the Study Area. The majority of it is residential. Basically, residential development is entering the market and will bring with it the base that commercial developers are looking for. The incoming residential development will help to diversify the current housing stock, as some is townhome and multi-family development. Office development is continuing, but it is starting to take a slightly different form, with multi-story buildings and office condos.

This is a relatively typical transition development pattern from exurban into suburban. As part of that transition, critical infrastructure investments have already been made, such as the expansion of the GA 400 exit at McFarland Road, and others are underway, including the northernmost segment of Ronald Reagan Boulevard. Additionally, a prototype of mixed-use development can be found just a short drive from the Study Area in Vickery. The success of this type of development is an indicator that new residents are coming into the area. It also is a sign that people are looking for a diverse set of choices and have discovered they might be able to find some of those in Forsyth County.

It is clear in 2005 that developers are actively pursuing this portion of the metro Atlanta market. There is considerable residential and commercial development underway, both in the Study Area and on its borders. The real challenge for the Study Area will be to harness its potential and guide its development in a strategic and focused way to ensure quality results and truly sustainable success.

While the Study Area is primarily an undeveloped site (or “greenfield”), there are numerous approved and pending developments within the McFarland–Stoney Point LCI Study Area. The table on the following page summarizes rezoning activity in the Study Area.

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ZA#	Status	Date Approved	Zoning District	Development Name *	Developer	General Location	Acreage	Type	SF detached # units	Proposed Development		Misc-use SF
										MF # units	Retail SF	
465	Approved	7/13/1981	M1	Corporate Campus The Meadows @ Bluegrass Comptable Campus @ The Meadows	Meadows Partners, LLC Ray Doud Cablex, LLC	Shiloh Road on both east and west sides	225	Office	-	-	-	-
766	Approved	7/23/1984	M1	McGinnis Ferry Office Park The Sales at McGinnis Ferry The Meadows Hub	Trautman Crow Company Mitschen Enterprises Focus	McGinnis Ferry Road and Union Hill Road	14	Office/Warehouse	-	-	-	-
1821	Approved	3/14/1985	CBD	-	-	McFarland Road and Shiloh Road	5.496	Hotel	-	-	-	135 rooms
1879	Approved	6/26/1985	CBD	-	-	McFarland Road and Bluegrass Valley Parkway	33.636	Mixed Use Retail and Hotel	-	225,000	-	75 rooms
2212	Approved	6/29/1988	M1	Buckhead Overlook	Wayne F. Orr John Moll	Shiloh Road	14.6	Office/Warehouse	-	-	224,500	-
2292	Approved	2/23/1989	M1	-	-	Shiloh Road	10.6	Office/Warehouse	-	-	37,500	-
2447	Approved	11/13/2000	M1	-	-	GA-400 and Majors Road	2.12	Light Industrial	-	-	9,300	-
2474	Approved	12/11/2000	R3	Majors Mill Crossing Oxford Summit	Trey Management Services, LLC Oxford Properties, LLC	Majors Road and GA-400	83.849	Townhome/MF	181	667	-	-
2606	Approved	11/12/2002	CBD	-	-	GA-400 and Union Hill Road	150.71	Mixed Use Retail, Office, Hotel, Residential	-	376	1,400,000	900,000
2616	Approved	7/8/2002	PUD	Ridge at Stoney Point	Reus-Forsyth, LLC McCar Development Corp. dba MDC Homes	Stoney Point Road	32.401	SF Residential	73	-	-	-
2635	Approved	10/14/2002	CBD	-	-	McFarland Road and Bluegrass Valley Parkway	2.1	Retail	-	-	6,640	-
2782	Approved	7/26/2003	R2R	Ridge at Stoney Point	Book Design Group McCar Development Corp. dba MDC Homes	Stoney Point Road	30.311	SF Residential	8	-	-	-
2862	Approved	11/22/2004	RE86	-	-	McFarland Road & Bluegrass Valley Parkway	66.77	Townhome/MF	-	300	-	-
2864	Approved	2/17/2005	RE86	The Summit at Shiloh Subdivision	Regent Land Holdings, LLC PCH Development, LLC	Shiloh Road	5.02	Townhome/MF	-	30	-	-
2865	Approved	2/17/2005	RE84	The Summit at Shiloh Subdivision	PCH Development, LLC	Shiloh Road	15.05	SF Residential	49	-	-	-
2867	Approved	2/17/2005	NS	-	Wayne F. Orr	Shiloh Road & Shiloh Road East	1.85	Retail	-	-	8,250	-
2915	Approved	1/10/2005	RE84	Provence Subdivision	Coordinated Properties, Inc., Fred Fliboff	Stoney Point Road and GA-400	120.61	SF Residential	347	-	-	-
3007	Approved	5/22/2005	RE84	-	Grant A. Grimes	Stoney Ridge Road	134.8	SF Residential	396	-	-	-
3020	Approved	9/1/2005	RE86	-	Highwoods Realty, L.P.	Bluegrass Valley Pkwy S and GA 400	42.88	Townhome/MF	-	267	-	-
3021	Approved	9/1/2005	RE83	-	Highwoods Realty, L.P.	McFarland Road	20.14	SF Residential	63	-	-	-
3060	Approved	8/16/2005	CBD	-	Greenstone Properties, Inc.	McGinnis Ferry Road and Union Hill Road	17.1	Mixed Use Office/Retail	-	-	-	270,000
3083	Approved	07/27/05	RE86	-	Grant A. Grimes	GA-400 near Stoney Ridge Road	11.98	Townhome/MF	-	63	-	-
3112	Approved	8/18/2005	RE86	-	Pope & Land Enterprises, Inc.	McGinnis Ferry Road	48.5	Townhome/MF	-	340	-	-
3121	Approved	9/15/2005	CBD	-	Highwoods Realty, L.P.	GA-400, McFarland Road and Bluegrass Valley Parkway	23.13	Retail	-	-	296,000	-
3149	Filed for the 12/15/05 BOC Meeting	On the Agenda for the 12/15/05 BOC Meeting	RE83	-	Waldrest Development, Inc.	Stoney Point Rd	19.428	SF Residential	49	-	-	-

*Note: () under Development Name indicates that a Site Development has not been filed or found for the ZA.

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Some of these projects, such as the “Ridge at Stoney Point,” have recently come to fruition. Others (those with a “Development Name,” including “Majors Mill Crossing” and “Provence Subdivision”) have attained Site Development Permits and construction is or will soon be underway. Of late, these have been strictly residential developments. The other projects may one day be developed, but not necessarily as originally proposed. Particularly for those projects yet to gain site plan permitting, there is an opportunity to guide them toward a more well connected and integrated center.

Since December 2000, applications have been processed for 18 parcels in the Study Area; again, though the timeframe of build out is undetermined, the following development has been approved:

<u>TYPE</u>	<u># UNITS</u>	<u>SQ FT</u>	<u>ACRES</u>
Single-family Detached	1,156 *	-	372.7
Townhomes / Multi-family	1,567	-	259.0
Retail	-	309,890	27.1
Mixed-use (Office / Retail / MF Residential) **	375	2,570,000	167.8
Total	3,098	2,879,890	826.6

** Also includes a 500 room hotel.

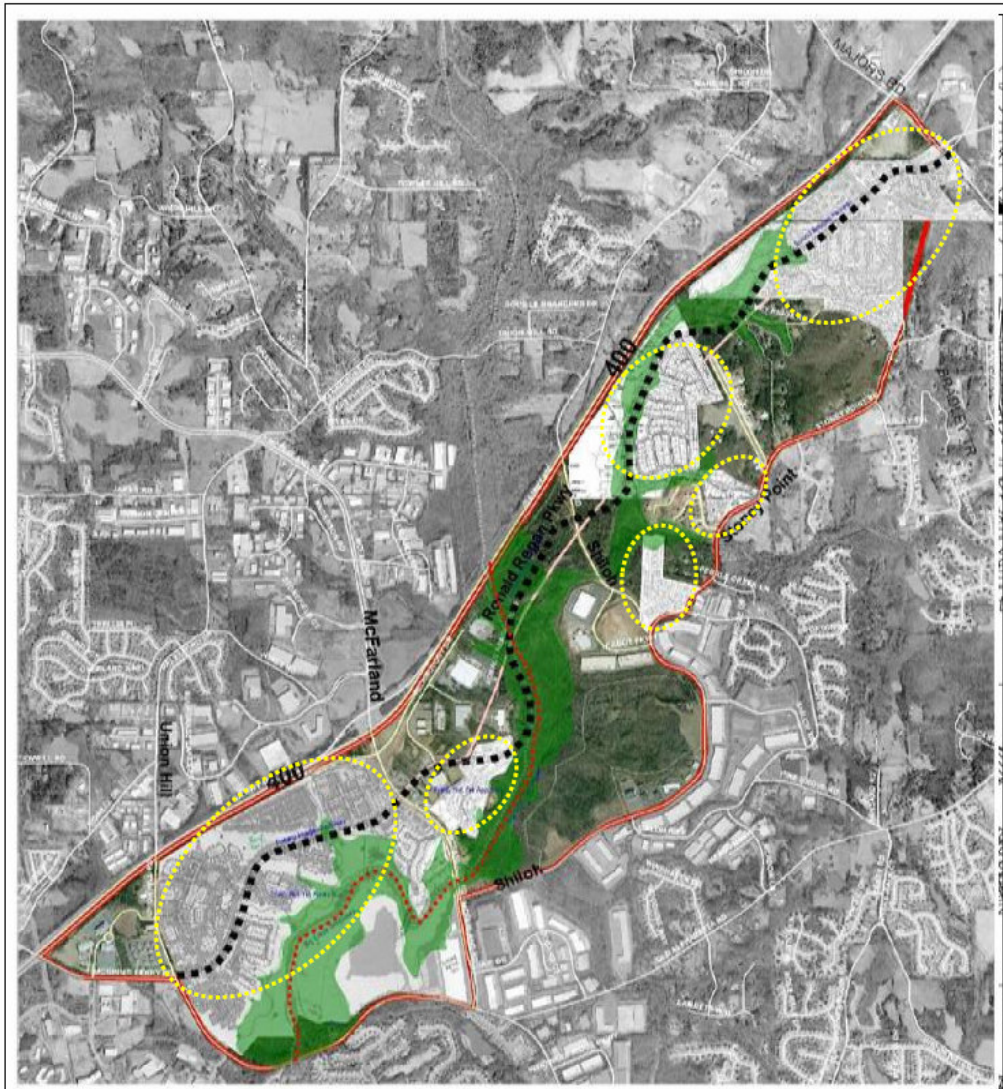
* These figures include an application for 49 units on a 19.4 acre site to be considered by the Board of Commissioners 12/15/05.

In general, these developments are not designed to interconnect as a cohesive unit. Though some of the approvals are conditional to dedication of right-of-way for (or, in some cases, partial construction of) Ronald Reagan Boulevard, this will not adequately serve inter-parcel connectivity. In the short term, local trips will be forced to use existing collector streets; Stoney Point Road and Shiloh Road will be overburdened until the new four-lane connects Majors Road to McFarland Road.

With the exception of ZA # 3149, which goes before the Board of Commissioners this month, the parcels displayed in the map on the following page have been approved for development. The subsequent map (A-9) compiles the proposed site plans made available to this study.

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Proposed Developments

Proposed Developments shown over Base Map

Forsyth LCI

Forsyth County, Georgia

Scale:



Map A.9



McFarland - Stoney Point Livable Center Initiative

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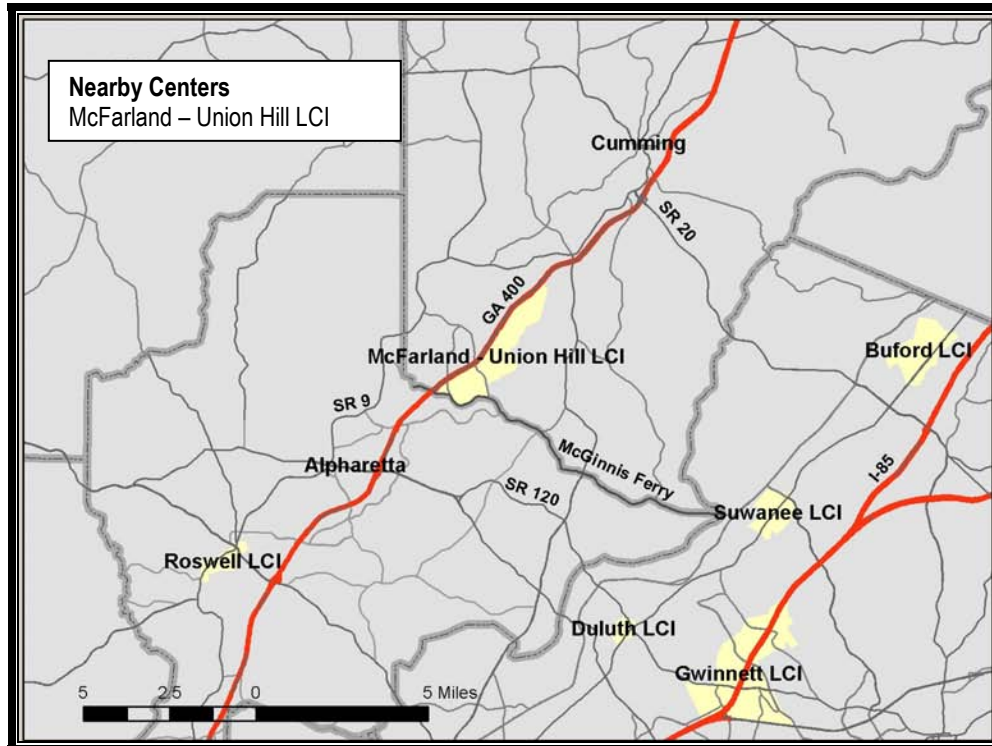


Transportation and Circulation Network

This section presents an assessment of the existing transportation facilities and circulation network in the area. Due to the present lack of a local street network, the analysis is presented with a more regional perspective.

TRANSPORTATION CONTEXT

The prominent aspect of the Study Area in terms of regional transportation context is its adjacency and access to GA 400, an ARC-identified cross-regional corridor. The cities of Cumming and Alpharetta are less than 5-miles to the north and south, respectively, on GA 400. The Study Area is within a 10-mile drive of three locations identified as livable centers, namely: Roswell, Suwanee, and Duluth. Though these are not directly linked to the Study Area by major routes, Suwanee is a straight shot east on McGinnis Ferry Road, segments of which are slated for improvements.



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RESIDENT COMMUTER CHARACTERISTICS

In order to assess the local transportation system, it is necessary to consider the relationship of the system to its users. Readily available data regarding localized travel behavior is provided by the 2000 US Census, which summarizes the commute behavior of working local residents.

For this purpose, the Study Area is statistically represented as the Big Creek Census Tract (CT 1306.00), which totally encompasses the Study Area. The table on the following page compares the commute characteristics of area residents with those of metro-Atlanta.

Key points include the following facts. Of commuters living in the area,

- nearly 3/4 work outside of Forsyth County (over half of metro-Atlanta commuters stay within their county of residence) ;
- less than 2% commuted to work by means other than private vehicle (vs. 6% of metro-Atlanta commuters) ;
- of those commuting by private vehicle, less than 9% carpool (vs. 13.6% for metro-Atlanta) ;
- 1 in 3 travel more than 45 minutes to work (1 in 4 of metro-Atlanta commuters) ; and,
- most all area households (98.5%) have access to a vehicle (7.3% of metro-Atlanta households do not).

In sum, if you live in South Forsyth and are employed, you spend a lot of time driving alone to work outside the county. This same travel character likely applies to other frequent destinations, such as shopping, recreation, and regular services.

There are two basic reasons for such auto-dependency. First is the distance separating preferred housing from common destination points; this is an urban design and land use issue addressed elsewhere in the plan. Second is the lack of facilities for alternate modes of transportation. For local planning purposes, a common issue for debate is whether area residents would use another mode if a viable alternative is offered. Presently, roadways dominate the local network. No transit serves the area; bicyclists must share roadway facilities which are not designed to accommodate them; and, pedestrian facilities which do exist merely serve trips within residential subdivisions. Clearly, no traveler can be expected to use an alternative mode if it is not offered.

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Commute to Work & Vehicle Availability

McFarland - Union Hill LCI Study Area

Forsyth County

Source: 2000 US Census. Note: "Big Creek" Census Tract 1306 completely encompasses study area

	Big Creek CT Total	Big Creek CT % or MEAN	Atlanta MSA % or MEAN
LOCATION OF EMPLOYMENT			
Worked in County of Residence	3,731	27.1%	50.8%
Worked outside County of Residence	10,033	72.9%	49.2%
Total workers 16 years and older	13,764	100.0%	100.0%
MEANS OF TRANSPORTION TO WORK			
Drove Alone	11,431	83.0%	77.0%
Carpooled	1,230	8.9%	13.6%
Sub-total: Private vehicle (car, truck, van)	12,661	92.0%	90.6%
Public Transportation (including bus, taxicab)	106	0.8%	3.7%
Walked	57	0.4%	1.3%
Other	93	0.7%	1.0%
Worked at home	847	6.2%	3.5%
Total workers 16 years and older	13,764	100.0%	100.0%
TRAVEL TIME TO WORK			
Less than 30 minutes	5,924	45.9%	50.7%
30 to 44 minutes	2,718	21.0%	25.1%
45 to 59 minutes	2,158	16.7%	12.4%
60 or more minutes	2,117	16.4%	11.8%
Total not working at home	12,917	100.0%	100.0%
Aggregate Travel Time to Work (minutes)	435,685		
Mean Travel Time to Work (minutes)	MEAN:	33.7	31.2
VEHICLE AVAILABILITY			
no vehicle available	130	1.4%	7.3%
1 vehicle available	1,299	14.3%	31.8%
2 vehicles available	5,524	60.9%	41.6%
3 or more vehicles available	2,116	23.3%	19.3%
Total occupied housing units	9,069	100.0%	100.0%
Vehicles per Household in Owner-Occupied Unit	MEAN:	2.2	2.1
Vehicles per Household in Renter-Occupied Unit	MEAN:	1.5	1.3

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ROADWAY FACILITIES

Roadways constitute the only viable existing transportation network serving the Study Area. An inventory of the Study Area roadways was created according to the degree to which they fulfill two purposes:

- movement of traffic, or mobility, and
- access to development.

As expressed in the following diagram, these functions are inversely related in that the more traffic volume a roadway can accommodate, the less access it provides (and vice versa).

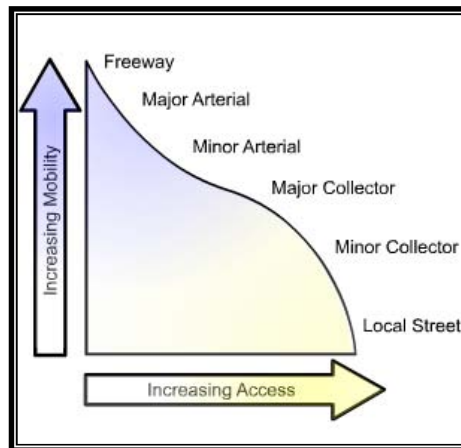


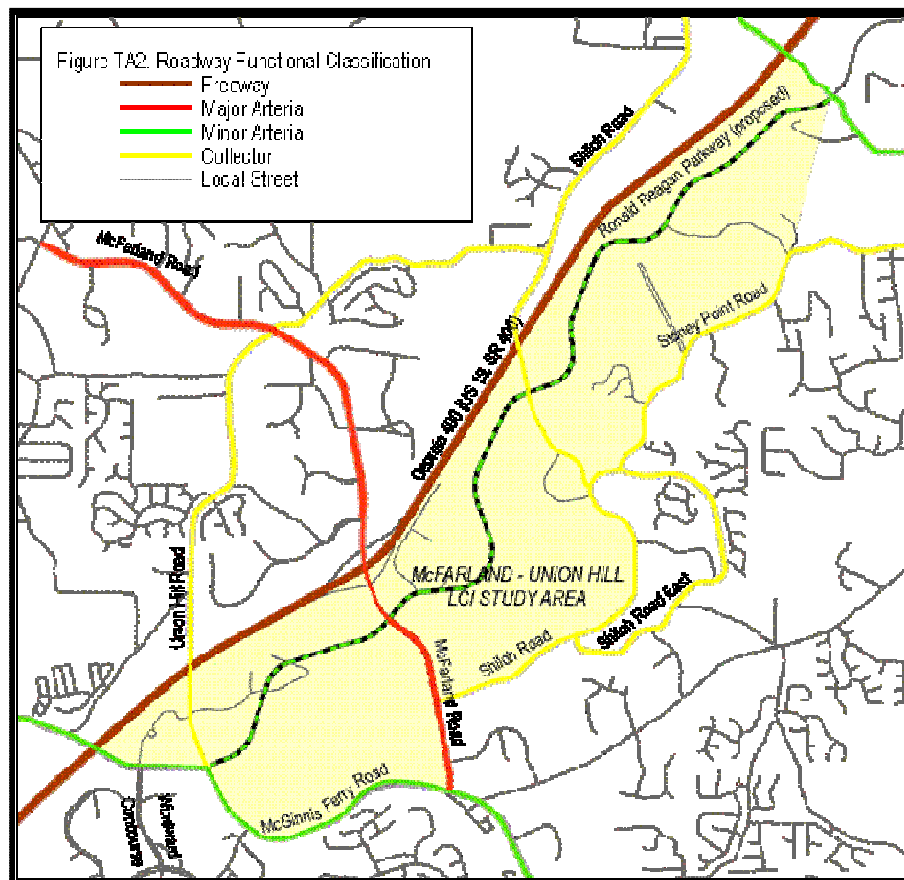
Figure TA-2. Roadway Classification by Function

Thus, a functional classification provides a hierarchical ranking based on the degree of mobility and accessibility that a street provides its users. The following Map and Table present an inventory of Study Area roadways by functional classification (as designated in the Comprehensive Plan). Note that many of these road segments merely run along the Study Area boundary including: GA 400, McGinnis Ferry Road, Stoney Point Road, Shiloh Road East, Majors Road, and parts of Shiloh Road.



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Route	County Route #	GDOT Functional Class		Forsyth County Classification	Miles in study area	# of lanes
		Urban / Rural	Type			
Georgia 400 (US Hwy 19, SR 400)	-	Urban	Principal Arterial	Freeway	4.57	4
McFarland Road	458	Urban	Minor Arterial	Major Arterial	1.13	4
McGinnis Ferry Road	283, 456	Urban	Minor Arterial	Minor Arterial	1.87	2
Stoney Point Road	52	Urban	Local Street	Collector	1.37	2
Union Hill Road	456	Urban	Local Street	Collector	0.46	2
Majors Road	19	Urban	Local Street	Minor Arterial	0.29	2
Shiloh Road	53	Urban	Local Street	Collector	2.41	2
Shiloh Road East	953	Urban	Local Street	Collector	0.16	2
Other Local Streets	-	Urban	Local Street	Local Street	4.77	vary
Ronald Reagan Parkway (proposed)	TBD	Urban	TBD	Minor Arterial	4.71	4



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Freeways, such as GA 400 (a.k.a. Turner McDonald Parkway), include limited access facilities, such as interstates, which serve high volumes of high speed through traffic. Freeways are designed for the purpose of mobility and have two or more lanes each direction separated by medians, within over 200 feet of right-of-way. GA 400 serves as the western boundary of the Study Area and plays a critical role in local travel behavior; however, an assessment of the operation of this freeway was outside the scope of this study.

Major Arterials, such as McFarland Road, connect the major activity centers of a metropolitan area and serve as corridors of high volumes. These routes primarily provide mobility through the region yet provide limited access to private property. A typical major arterial is a 4- to 6-lane highway divided by a median, within 100-200 feet of right-of-way.

Minor Arterials, such as McGinnis Ferry Road and Majors Road, are those routes which serve the dual purpose of mobility and access. Such roadways may carry 20,000-50,000 vehicles daily and have anywhere from 2-5 lanes within 80-120 feet of right-of-way, yet also provide direct access to local streets and commercial driveways. Local convention does not differentiate major collectors, thus they tend to be classified as minor arterials. Note that Ronald Reagan Boulevard as proposed will function as a minor arterial.

Collectors, such as Union Hill, Shiloh, and Stoney Point Roads, are intended to provide access to arterials from local streets and commercial driveways; thus, they are more frequently intersected. Direct access onto Collectors from Single-family driveways is discouraged. Such streets typically have 2-4 undivided lanes within 60-100 feet of right-of-way and carry 7,500-15,000 vehicles daily.

Local Streets, which include both neighborhood streets (Huddlestone Way) and commercial streets (Windward Concourse, Bluegrass Valley Way, Technology Drive, and Cabot Parkway), provide direct access to the roadway network from development. They typically have just 2-lanes within 80 feet of right-of-way or less. High volumes are discouraged and on-street parking may be appropriate in activity centers. Local streets which act as “cut-through” routes are candidates for traffic calming.

Forsyth County’s Comprehensive Plan recognizes the GA 400, McFarland Road, and McGinnis Ferry Road Corridors as playing increasingly important roles in accommodating anticipated development in South Forsyth in the future.

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ROADWAY LEVEL OF SERVICE

Important measures of roadway traffic are volume (number of vehicles using the road) and capacity (number of vehicles a road is designed to serve). These measures can be used to quantify a roadway's level of service (or level of congestion) at various times of day.

Due to the Study Area's rural character and the limited scope of this analysis, there is limited historic or present day traffic volume data available. It is clear that anticipated new development, new roads, and improved roadway facilities in the area will significantly alter current traffic counts and travel patterns. In order to predict where to invest in capital improvements, it is necessary to estimate where congestion points may be in the future. Though it does not include local streets or collectors in the area, the ARC-maintained regional transportation model is a readily available means to forecast future volumes.

The following table offers an analysis of the available traffic count data and ARC forecasts for Study Area roads (*GA 400 data excluded*). Volume is reported in Average Annual Daily Traffic (AADT) which represents the number of vehicles traversing a particular road segment in either direction in a typical 24-hour period.

Road Name	Past Volumes (GDOT)							Future Volumes (ARC)	
	1997 AADT	97-00 annual change	2000 AADT	00-03 annual change	2003 AADT	03-04 annual change	2004 AADT	forecast annual change	2030 AADT
McFarland Road	9,945	13.4%	14,500	12.7%	20,760	4.3%	21,660	1.4%	31,170
McGinnis Ferry Road	-	-	-	-	10,370	-	-	2.3%	19,220
Stoney Point Road	-	-	-	-	-	-	1,580	-	-
Union Hill Road	-	-	-	-	-	-	-	-	7,160
Majors Road	-	-	-	-	-	-	-	-	17,930
Ronald Reagan Parkway	-	-	-	-	-	-	-	-	10,640

This data alone is insufficient to draw major conclusions. However, the increase in demand on McFarland Road since the late 90s is well documented and is representative of the increasing pressure on the local street network. Note that the ARC forecasts account for capacity increasing projects (described in a later section) on all of these routes except Stoney Point Road.

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A helpful means of identifying potential congestion points is offered by the ARC model. The maps on the following page offer a thematic representation of existing (2005) and future (2030) congestion during the evening rush hour, or "PM Peak." Not unlike school grades, grades of A through F are assigned based on the ratio of vehicles to capacity (or V-C ratio) for each roadway segment. Levels of service A-C are "passing" (even if minimally), suggesting levels of congestion deemed acceptable by regional standards. Level of service D suggests unacceptable congestion. Levels of Service E-F suggest what amounts to operational failure - unacceptable levels of delay and congestion each afternoon at rush hour.

In reference to the 2005 graphic, areas of notably levels of congestion include:

- GA 400 Northbound ;
- McGinnis Ferry Road between Bethany Road and Union Hill Road ; and
- the McGinnis Ferry Road / McFarland Road intersection in general.

The 2030 model not only introduces Majors Road and Ronald Reagan Boulevard, but also incorporates numerous improvements to the area road network including:

- widening of McFarland Road to 6 lanes ;
- widening of Union Hill Road to 4 lanes ; and
- widening of McGinnis Ferry Road to 4 lanes.

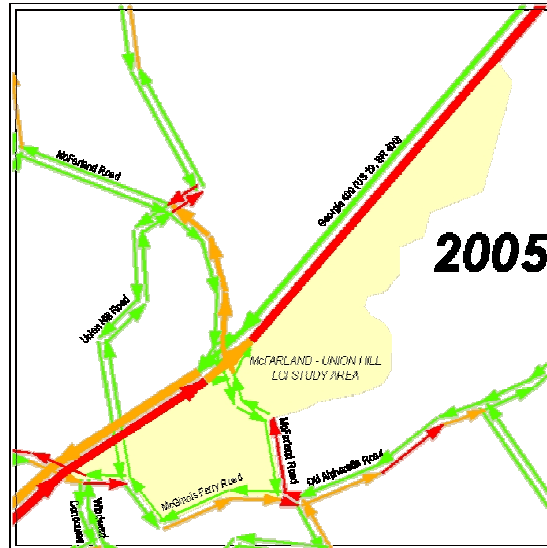
(An interchange at McGinnis Ferry Road and GA 400 is not included in the model). Despite these improvements, the model forecasts unacceptable congestion levels for the following road segments:

- GA 400 Northbound ;
- McGinnis Ferry Road between Bethany Road and Union Hill Road ;
- McGinnis Ferry Road between McFarland Road and Old Alabama Road; and
- Majors Road between SR 9 and SR 141.

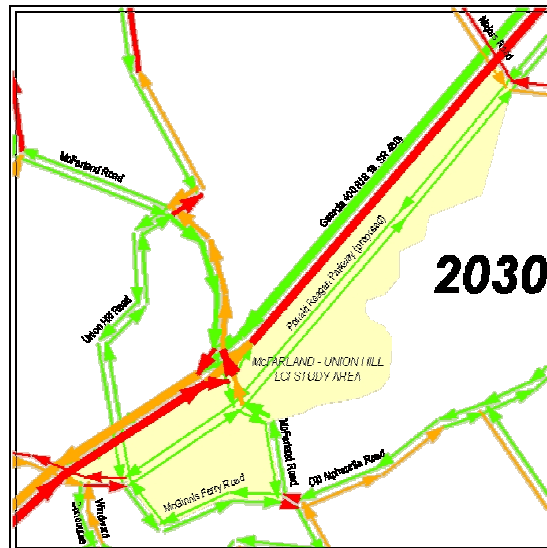
Though Ronald Reagan Boulevard is forecast to serve at a passing level of congestion, it is important to note that it is anticipated to carry over 10,000 vehicles daily. The boulevard must be carefully designed and managed to achieve this volume without hindering development of the area as a pedestrian-friendly activity center.

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2005 CONGESTION



2030 CONGESTION

Vehicle to Capacity Ratio at PM Peak

LOS A-C	LOS D	LOS E-F
Volume < 5,000	Volume < 5,000	Volume < 5,000
Vol. 5,000 - 10,000	Vol. 5,000 - 10,000	Vol. 5,000 - 10,000
Volume > 10,000	Volume > 10,000	Volume > 10,000



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BRIDGES & CULVERTS

Forsyth County's Comprehensive Plan rates all area bridges and culverts as in "good" condition except for the structure supporting McGinnis Ferry Road over Big Creek which is reportedly in "fair" condition. Due to the traffic interruptions which would be required for the improvement or replacement of the culverts at this location, the work should not be programmed until Ronald Reagan Boulevard provides a temporary alternate route, if possible.

ROADWAY SAFETY

Another means of assessing roadways is through analysis of traffic accident data. The following table summarizes three years worth of GDOT accidents reported in the Study Area (2002-2004).

Route / Location	Miles	Accidents	Injuries	Fatalities
McFarland Road	1.18	114	35	0
Shiloh Road	2.39	22	11	0
Union Hill Road	1.89	12	3	0
Other Local Roads	2.56	8	7	0
Totals	8.02	156	56	0

On average, an accident occurred every week in the area and someone was injured in about one of every three accidents. McFarland Road was by far the most accident prone section of roadway in the Study Area over the years 2002-2004. However, considering the volume of cars on McFarland Road during that time period, there were relatively fewer accidents than on comparable arterials throughout the state. The following table relays this comparison:

Route	Annual Accidents per 100 million Vehicle Miles		
	All Accidents	Injury Accidents	Fatal Accidents
McFarland Road ('02-'04)	425	130	0.0
Statewide Minor Urban Arterials ('03)	572	218	1.5

This relatively low accident rate may be attributable to design. In 2002, McFarland Road was widened from 2-lanes to a divided 4-lane. GDOT asserts that medians control turning movements, which reduces potential points of conflict, and thus improves safety.

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INTERSECTIONS, SIGNALIZATION, & SIGNAGE

Roadway intersections are important nodes of connectivity.

All currently signalized intersections in the Study Area are located on McFarland Road at intersections with the following roads:

- GA 400 (northbound access),
- Bluegrass Valley Parkway (Future Ronald Reagan Boulevard),
- Shiloh Road,
- Curie Drive, and
- McGinnis Ferry Road.

Other key intersections not currently signalized (listed from south to north) include:

- McGinnis Ferry Road at Windward Concourse,
- McGinnis Ferry Road at Union Hill Road (1 of 2, future Ronald Reagan Boulevard Tie-in),
- McGinnis Ferry Road at Union Hill Road (2 of 2, potential Greenway Trailhead and access to Union Hill Park in Alpharetta),
- Union Hill Road at Windward Concourse,
- Shiloh Road at Shiloh Road East (1 of 2, just north of McFarland Road),
- Shiloh Road at Shiloh Crossing (a 3-way stop),
- Shiloh Road at Shiloh Road East (2 of 2, where Shiloh Road turns left/north), and
- Shiloh Road East at Stoney Point Road (a 3-way stop).

Notably, numerous subdivision access streets (some under construction) access Stoney Point Road. Some of these neighborhood streets are cul-de-sacs and many are not aligned across Stoney Point.

When constructed/improved, the following intersections will be critical in the future:

- McGinnis Ferry Road at GA 400 (Long-term Proposed Interchange),
- Ronald Reagan Boulevard at Shiloh Road, and
- Ronald Reagan Boulevard at Majors Road.



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PEDESTRIAN FACILITIES

Forsyth County ordinances have not historically regulated sidewalk installation in private developments; thus, provision and connectivity are poor. Area commercial and industrial developments cater to automobile access. Area subdivisions tend to provide limited sidewalks from residences to private recreation facilities (e.g. neighborhood pools or tennis courts).

The only existing pedestrian facilities in the Study Area are:

- Sidewalks on both sides of McFarland Road (< 1000 ft from GA 400 to Bluegrass Valley) ;
- Signalized Crosswalks (4-way) at McFarland and Bluegrass Valley Parkway ;
- Signalized Crosswalks (2-way) at McFarland intersections with GA 400 NB Access, Shiloh Road, Curie Drive, and McGinnis Ferry Road ;
- Sidewalks on one side of subdivision streets (Huddlestone Way and others, < 3000 ft total).

Field observations confirm demand for pedestrian facilities on and within the Shiloh Road East loop (as identified in the County Bicycle and Pedestrian Plan).

BICYCLE & SHARED-USE FACILITIES

No dedicated bicycle facilities, bike lanes, multi-use trails, nor shared-use paths currently serve the Study Area.

Arguably, some of the old unpaved 1-lane drives in the area (including Union Hill Drive, Tom Wallace Drive, Stoney Ridge Drive, and Old Johnson Road) carry such low volumes of traffic that they are friendly to off-road bikes or hikers. However, these merely provide access to rural acreage and do not constitute a network. Further, development plans do not propose that these routes be maintained.

Though US Census data suggests that local residents do not commute to work by bicycle, site visit observations suggest that recreational cyclists find the area attractive, presumably for the open space vistas.

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PARKING, RAIL, AND AIR

Beyond surface parking, there are currently no parking structures, park and ride lots, fixed route transit, local transit routes, or rail lines serving the Study Area.

Forsyth County operates an “on-call” Dial-A-Ride program; three vans are made available to residents on an appointment basis for \$1.50 to \$4.50 per trip.

There is a private airfield, “Kitfox Airstrip,” off of Stoney Point Road which caters to small aircraft. No expansion of this facility is anticipated.

TRANSPORTATION ISSUES

Community residents identified the following as priority issues:

- Stoney Point Road is overburdened by high volumes of cut-through traffic and is not safe for pedestrians.
- Some area roads do not have adequate shoulders to accommodate pedestrians; these require sidewalks.

Following are key transportation issues:

- Lack of Infrastructure. The existing transportation infrastructure is not sufficient to support an activity center. Without new and/or improved facilities, anticipated development will clearly overburden existing arterials (McFarland Road, McGinnis Ferry Road, and Majors Road) and collectors (Union Hill Road, Shiloh Road, and Stoney Point Road).
- Alternative Transportation. Existing public facilities and private development predominantly cater to motorized vehicle mobility and access. Limited existing facilities serve alternative modes. Multi-modal choice is a new concept for the community.
- Ronald Reagan Boulevard Corridor. The alignment, functional purpose, and design character of this proposed 4-lane divided roadway must be addressed. A challenge will be

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to incorporate community goals into decisions regarding this facility, segments of which are presently being funded and constructed by multiple private interests.

- Pedestrian Environment and Streetscape Aesthetics. New investment in the transportation network should honor goals beyond mobility and access to focus on enhancing the pedestrian experience, especially in areas identified by the community as suitable for more intense development.
- Access-related Land Use and Development Regulations. Zoning, land use, and development regulations must specifically address right-of-way and inter-parcel connections for both automobiles and pedestrians (including design standards for curb-cuts, parking, building orientation to streets, walkway connectivity, etc). These should be implemented to preserve the functionality of public infrastructure.



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Demographics, Socio-Economics, and Employment

The following section discusses demographics, socio-economics, and employment trends and forecasts for the region, market areas, and to a lesser extent the Study Area itself.

Regional Population and Employment Trends

As the Atlanta Regional Commission (ARC) has documented, the Atlanta Region experienced dramatic and consistent growth during the 1990s. Between 1990 and 2000, the Atlanta Region grew by 34%, averaging to an annual growth rate of 3.4%, or adding about 87,000 new residents per year. The Atlanta Region was able to move out of the recession of the early 1990s pretty quickly, based on a diversified economic base. In fact, the Region nearly doubled its size between 1980 and 2005, when its total population reached about 3.8 million. The Atlanta Region experienced a similar phenomenon in job growth, more than doubling during the same time period, to about two million jobs. It is widely known that Atlanta's population growth has been fueled primarily by people moving to the Region for jobs. As the national recession has slowed job growth, so has the Atlanta Region seen a slowdown in population growth.

Historically, most of the growth within the Region was seen in more suburban locations. During the 1980s and 1990s, the north side of town experienced roughly 75% of the Region's total growth. In terms of employment, most of the Region's job growth happened along the GA 400 corridor, in the Perimeter Center area, and in northern Gwinnett and Fulton counties. Since the mid-1990s, growth has accelerated on the south side (with I-20 as the demarcation line) as congestion has increased and land has become more expensive on the north side. The Region's areas with the greatest population increases between 2000 and 2004 are all located in counties outside I-285: northern and eastern Gwinnett, north Fulton, northwest Cobb, south DeKalb, central Cherokee, and Henry.

The more urban counties in metro Atlanta have continued to add new residents, but their overall population share has declined relative to further-out counties. Incorporated cities in the Region account for less than a third of the region's population gains between 2000 and 2004. Population density across the metro area continues to be low, in comparison to other large metropolitan cities, but it is increasing.



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The expectation across the Region is for growth to continue, both in population and employment, but at slower rates than the enormous expansion that was seen during the 1990s. Jobs are expected to increase by 1.2 million by 2030. Population is expected to increase by 2.3 million by 2030. Net immigration is expected to account for just over half the growth in the Region. Suburban counties are expected to experience the highest growth rates over the next 25 years, in terms of both population and employment. The Atlanta Regional Commission's forecasts indicate that the Region's economy will still outpace the nation in terms of growth, even though we will not see the phenomenal rates of growth that were experienced in the late-1990s.

Forsyth County Population and Employment Trends

Cumming, Forsyth's county seat and only city, was the historical center of activity and employment in the county. However, population and employment growth are clearly following the GA 400 corridor out of North Fulton into Forsyth County, trending toward a less centric and more linear pattern of development. According to ARC forecasts, Forsyth County's population and employment are both expected to increase by more than 150 percent to total 264,422 persons and 93,346 jobs in 2030.

South Forsyth Population and Employment Trends

South Forsyth County is statistically defined as the Big Creek Census Tract (CT 1306.00) which encompasses the Study Area. By 2000, this tract was the county's largest in terms of both employment (13,914) and population (27,065). With its relatively large land area (44.3 square miles) and direct access to GA 400, the tract is forecast to grow rapidly. In 2030, it will still be the location of more jobs, (28,887), and population (55,420) than any other tract in Forsyth County, according to the ARC.

Study Area Population and Demographics

Readily available information about residents of the Study Area is limited to the data obtained by the US Census in 2000. At that time, there were just 164 households in the entire 2,150 acre Study Area (.08 households per acre). Households were large by present metro-Atlanta standards (3.23 persons per household) and predominantly owner-occupied (96.3%). The Study Area population

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was 529, of whom 93.6% were white and non-Hispanic. The area might be simply described as agricultural open space (4.06 acres per resident).

Population (2000)	529 residents	
Total Study Area Acreage (GIS)	2149 acres	
Residential acres	306.48 acres	
Overall Population Density	0.25 people/acre	
Residential Population Density	1.73 people/residential acre	
Overall Household Density	0.08 households/acre	
Household Density	0.54 households/residential acre	
average total acres per person	4.06 total acres/person	
average lot size (residential acres per household)	1.87 residential acres/household	
	# in LCI	% of LCI total
Children (<18)	179	33.8%
Adults (18+)	350	66.2%
Total Population (2000):	529	100.0%
White (Non-Hispanic)	495	93.6%
Black (Non-Hispanic)	0	0.0%
Other (Non-Hispanic)	17	3.2%
Non-Hispanic	512	
Hispanic or Latino (regardless of race)	17	3.2%
Total Population (2000):	529	100.0%
Total Households (2000)	164	
Average Pop per household	3.23	
Owner-Occupied	158	96.3%
Renter-Occupied	6	3.7%
Occupied	164	100.0%
Occupied	164	96.5%
Vacant	6	3.5%
Total Housing Units (2000)	170	100.0%

Note that due to statistical limitations, it is necessary to discuss socio-economics of the Study Area at a broader geography than just the Study Area. Thus, hereafter, the Study Area is statistically represented as Census Tract 1306, Block Group 1 (as defined by the 2000 US Census).

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The Study Area has grown approximately 180% since 1990; clearly demonstrating that the area has received a significant share of the phenomenal growth the Atlanta Region saw during this time. Between 1990 and 2000, the Study Area experienced its most significant growth, indicating the transition it was undergoing from rural to exurban. Since 2000, the Study Area has continued to transition, from exurban to suburban. The population growth in the Study Area since 2000 is three times the growth seen in the 20-county Atlanta MSA as a whole. The growth expected over the next five years is on pace with Forsyth County, but is three times the Atlanta MSA average, and six times the national average.

	1990	2000	CENSUS-BASED				ARC	
			2004	2009	Change 1990- 2004	Change 2004- 2009	Change 2000- 2005	Change 2005- 2010
Study Area	5,047	10,412	13,961	18,117	176.6%	29.8%	11.0%	9.8%
Primary Market Area	16,183	49,950	64,964	81,336	301.4%	25.2%	N/A	N/A
Secondary Market Area	159,924	332,447	383,751	440,882	141.5%	14.9%	N/A	N/A
Atlanta MSA/Region	2,959,980	4,112,224	4,570,418	5,101,784	54.4%	11.6%	6.0%	7.7%

Census-based statistics primarily use a straight-line projection methodology based on historic trends. This does not always paint an accurate picture of what is actually happening in a community, but is usually better suited for rapidly suburbanizing areas like the Study Area. Estimates and forecasts from the Atlanta Regional Commission were also reviewed. ARC's projections provide a local perspective on what is happening in the Study Area, as people continue to move further out. An annual household growth was determined using a combination of Census-based and ARC forecasts. Combining the two ensures a mid-range approach, instead of being too conservative or too aggressive; this is what is used as the basis for forecasts throughout these analyses.

While employment growth is projected to be moderate for the Region, it is expected to be witnessed in existing employment centers. The location of the Study Area in the GA 400 corridor is well-positioned to receive job growth as more employers continue the natural trek northward up GA 400 from Perimeter to Sandy Springs to North Point to Windward, and eventually to McFarland. The

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daytime population within the Study Area is moderate, but when considering the Primary Market Area, that number jumps incredibly. The Primary Market Area includes parts of Windward Parkway and Johns Creek, two significant employment centers for the Atlanta region. The impact of the surrounding development on the Study Area is apparent when considering the needs for convenience and retail goods and services.

	Study Area	Primary Market Area	Secondary Market Area
Daytime Population	12,254	42,181	173,074
Businesses	1,158	3,436	16,959

Employment

Economies do not function locally, economics is a regional phenomenon. Trying to isolate detailed employment numbers and still retain meaning for them is a difficult endeavor. Sector employment and business size data are collected through different methodologies, thus different geographies need to be used for comparison. In terms of sector employment, the 30005 zip code reports Professional Services as its largest employment sector. There are three sectors that essentially “tie” for second largest: Retail Trade, Finance/Insurance and Real Estate, and Manufacturing. The largest employment sectors for 30005 zip code are generally the highest-paying sectors, with the exception of Retail Trade. It is interesting to observe that the largest employment sector for the MSA, Educational and Health Services, comes in fifth in size for 30005. It is also worth noting how small the Construction sector is in 30005 zip code, just about a quarter of the proportion found in the MSA.



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Industry Sector	30005 Zip Code	Atlanta MSA
Construction	2.8%	8.0%
Manufacturing	12.1%	11.1%
Wholesale trade	5.4%	4.5%
Retail trade	12.4%	11.9%
Transportation and warehousing, and utilities	4.7%	6.9%
Information	9.4%	5.0%
Finance, insurance, real estate, and rental and leasing	12.2%	8.0%
Professional, scientific, management, administrative services	18.7%	12.6%
Educational, health and social services	11.5%	15.6%
Arts, entertainment, recreation, accommodation and food services	6.1%	7.2%
Other services (except public administration)	3.0%	4.7%
Public administration	1.5%	4.2%

Workers in the 30005 zip code are predominately employed in management and professional occupations, with sales and office occupations and service occupations following as second and third. These rankings mirror the Atlanta MSA. However, it is interesting to note that the 30005 zip code has over 20% more people in management and professional occupations than the MSA, this is important because these are usually high-paying jobs.

Socio-Economics

As mentioned earlier, across the Atlanta Region, there has been a continuing push to develop further out. As development happens in previously rural areas, many people continue to move outward in an effort to recapture some of those elements. The Study Area was cited again and again as having many of these features that people find enticing, all while having access to the metro area's assets as well. People like the rural feel, the greenspace and open space that still exist, the small community feel, the remnants of horse farms and larger estates, lower taxes, and good schools. Conversely, much of the Region's development is clearly headed this way, and much has already arrived in the Study Area. There are congestion and traffic issues that are facing both residents and employees trying to access the area. Land prices are increasing steadily as more and more people discover the area. Amenities and services are lacking in close proximity to the Study Area.

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The following table illustrates the key demographic and economic elements of the markets being considered in this study.

Study Area	Primary Market Area	Secondary Market Area
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SIZE OF MARKET

Residents	13,961	64,964	383,751
Households	4,616	24,411	148,469
Daytime Population	12,254	42,181	173,074

CHARACTERISTICS OF MARKET

AGE			
Under 18	31.0%	31.1%	29.8%
Between 25 & 35	20.0%	22.0%	21.6%
Over 65	5.5%	5.4%	6.4%
INCOME			
Per Capita Income (PCI)	\$33,268	\$38,268	\$36,163
PCI as % of National Average	141.5%	162.8%	153.9%
Change in PCI since 1990	108.2%	111.7%	66.2%
Household Incomes \$25,000 - \$49,999	14.9%	15.7%	18.1%
Average Household Income	\$100,617	\$102,780	\$93,353
HOUSEHOLDS			
Average Household Size	3.02	2.65	2.57
Single-Person Households	12.0%	18.5%	20.3%
Owner-Occupied Households	89.4%	76.7%	72.1%

PROJECTED GROWTH OF MARKET

Census-Based, 2004-2009	29.8%	25.2%	14.9%
ARC, 2005-2010	9.8%	N/A	N/A

Elements that deserve specific highlighting include the following:

- The Study Area is expected to grow on pace with the phenomenal growth seen in Forsyth County as a whole, 30% between 2004 and 2009. This is over three times the national average.

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- The Atlanta Regional Commission's and the Census-based projections are quite different. The Atlanta Regional Commission has a larger starting residential base, but projects it at a slower growth rate while the Census-based projections show a much higher projected growth rate.
- The three largest age groups in the Study Area are 35 to 44, 25 to 34 and 5 to 13 years of age. These statistics demonstrate young families and people starting families in the area. The median age of the Study Area is 33.0, about three years younger than the national average.
- There are few college and early professional-aged residents, and a small proportion of retirement aged people in the Study Area. However, the largest growth in the next five years is expected in the age group of 65 to 74 (59.3%), which is currently less than half the national average. In terms of recent growth, in the last five years, the 18 to 24 age group was one that saw the largest increase, so this group is growing, and will continue to.
- The proportion of college graduates in the Study Area (35.5%) is above the national average (24.5%) and just above the Atlanta MSA average (31.6%).
- The per capita income (perhaps the most important statistic to review in terms of understanding how a community is really doing) in the Study Area (\$33,268) is 141.5% of the national average, a difference of about \$9,700 annually. The impact of suburbanization and the development pattern moving north up GA 400 is evident when considering the Study Area's PCI almost doubled between 1990 and 2000, and has continued to grow steadily. The projected growth in the next five years for the Study Area's PCI is four times the Atlanta MSA average.
- The Study Area's household income brackets below \$25,000 have been rapidly declining since 1990, and are expected to continue to decline in the future. Nearly three-quarters of the households in the Study Area earn above \$50,000 annually (compared to 46% of the nation). The proportion of Study Area residents that earn over \$100,000 (36.5%) is more than double the national average (14.8%) and over one and a half times the Atlanta MSA average (20.6%).
- The average household income in the Study Area is \$100,617, which is greater than both the national (\$61,246) and Atlanta MSA (\$72,403) averages.

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- The ratio of single-person households in the Study Area (12.0%) is about half the national (23.7%) and Atlanta MSA (22.8%) averages. This is important to note, as many times it can be an indicator of the young professional population.
- The average household size of the Study Area is above both the national and Atlanta MSA averages.
- The Study Area has a greater proportion of owners than both the national and Atlanta MSA averages.

There is opportunity for the demographic profile and trends to change as continued development takes place. The potential types of uses that are supportable in this market can go in two distinct directions: a more traditional, conservative development scenario that is predominately residential with few amenities and services that are accessible only by driving; and a mixed-use development scenario, including both residential and commercial, which will help to make the area more of a destination.

Additional detailed demographic and economic information can be found in the Appendix.



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Market Analysis

After first establishing the market context, the Market Analysis is broken up by land use into Residential, Retail, Office, and Industrial Submarkets.

Market Context

To determine the potential for new uses or support for existing and expanding uses, it is important to first understand who the market is. Understanding the demographic and economic characteristics of the residents and workers in the area is critical in understanding why the market is where it is, how the market can develop, whether it is under-served or saturated, and what would be supportable. It is also important to review the historic trends that have occurred in the area, as well as considering what is currently being projected to happen in the area in the future. All of these characteristics go into formulating what kind of development can be supported and how much can be supported. While the numbers begin to craft the backdrop for the story of the Study Area, they certainly can not effectively convey the entire story. The final recommendations will be based on a mixture of quantitative and qualitative analyses. Maps of these areas are on the immediately following pages.

Primary Market Area. The Primary Market Area is defined by a 10-minute drive time from the intersection of Cabot Parkway and Shiloh Road. On average, residents are willing to drive less than ten minutes (usually between two and three miles) for convenience retail, such as groceries, sundry items, dry cleaners, etc. This drive is usually at the maximum of this range for suburban/exurban areas where uses are traditionally more spread out. This area is primarily comprised of residents of the immediate area, or workers from businesses located in the area, in search of convenience-related goods and services. Restaurant customers would most likely be those making spontaneous decisions to eat out or pick something up for dinner that evening.

Secondary Market Area. The Secondary Market Area is defined by a 20-minute drive time from the intersection of Cabot Parkway and Shiloh Road. This area is where the majority of customers will come from. These consumers will be looking for some convenience retail, but will also be searching

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Residential Market Analysis

OVERVIEW

Housing sales and values in metro Atlanta remain strong. The Atlanta MSA led the nation again in residential permits in 2004, with 74,007; it is basically on pace with itself, compared to this time last year. The median sales price for single-family homes in the Atlanta MSA was at \$166,500 for second quarter 2005, according to the National Association of Realtors. This is following the national trend of a record high in housing prices. The appreciation of home prices, relative to inflation, are the strongest increases on record, and demonstrate the continuing issue of housing shortages across the nation. The Atlanta MSA median sales price gained 6.2% since this time last year. Condominium sales are 6.4% higher than this time last year. The median sales price in the Southern United States is \$208,800 for condos, up 24.0% from 2004.

Multi-family housing is making a slow, but steady resurgence. The effects of the recession and job losses are still evident, as people moved or entered roommate situations to alleviate impact. The historically low interest rates that have been seen in the last few years served to make renters into first-time homebuyers. As interest rates continue to increase, and the economy rebounds, more potential renters will emerge, creating a higher demand for rental housing. Some increases in inventory through new construction are already evident in the metro area.

There are some concerns that the pace of building in metro Atlanta is actually slightly outpacing sales; combined with rising interest rates and a slowing in population growth as a result of the recession, this could indicate some slowing in the rate of home value appreciation, as well as overbuilding. While home values seem to be appreciating at a slower rate, it appears to be helping to stabilize the market, and there is not an expectation for a significant crash in the housing market, but perhaps a slight softening. With that said, home values and sales located either very close in or very far out seem to be pushing past this slowing trend. Many realtors and brokers indicate pent-up demand at each end of the spectrum -- in urban, close-in locations as well as those in relatively undeveloped exurbs that still retain a rural feel.

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Forsyth County has been experiencing a housing boom the last few years, counter to some of the housing trends being seen in other parts of metro Atlanta. Residential growth has quickly increased, along with housing prices. Some zip codes in Forsyth County led metro Atlanta in terms of highest home sales prices and highest new home sales. However, this area of Forsyth County has experienced a slightly different circumstance. There is only a small proportion of residential product currently in the Study Area, though there have been many development applications recently that will increase residential development significantly.

BUILDING PERMITS

Residential building permit activity in Forsyth County has been consistently strong since 2000. Annual building permits for single-family residential have been at or above 2,500. The performance for 2005 year-to-date is definitely on a trend-line to surpass total residential building permits for last year, and looks to be headed for the highs seen in 2000 and 2003. The last multi-family residential building permits were issued in 2000. Since that time, there has been no multi-family construction activity in Forsyth County.

Within the Study Area, residential building activity had been consistent, but low. The Study Area accounted for only .16% of the residential building activity for all of Forsyth County in 2000. Commercial building activity had been more active within the Study Area since the mid to late-1990s. However, residential building took a notable upswing in 2004. At that time, the Study Area accounted for just over two percent of residential building within Forsyth County. It is evident that residential building has strongly entered the Study Area, and will probably continue at a steady pace for at least the next few years.

ZIP CODES

An interesting characteristic of the Study Area is that it is serviced by two zip codes: 30005 and 30041. This is an important characteristic because there are differences in value in each zip code due to of development patterns and what other areas are included. For instance, 30005 crosses into North Fulton County and includes much of Alpharetta and the Windward Parkway area; while 30041 is more of eastern Forsyth, including the Peachtree Parkway area. The 30005 zip predominates the Study Area, while 30041 is a small, but important, portion in residential terms.

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The median sales price (\$334,900) for the 30005 zip code has appreciated since 2000, posting a gain of 25% in overall value. The largest increase was seen in new home prices, which grew by 94%. Existing home sales prices also increased during this timeframe, by about 26%. The 2005 median sales price for new home sales is \$518,950, and \$333,950 for existing or re-sales. It is important to remember that this is a median price, not an average. The median value represents a mid-point in the range of lowest and highest values found in the zip code.

The median sales price (\$268,600) for the 30041 zip code has increased since 2000, posting a gain of 40% in overall value. The largest increase was seen in existing home prices, which grew by 22%. New home sales prices also appreciated during this timeframe, by about 46%. The 2005 median sales price for new home sales is \$285,200, and \$240,750 for existing or re-sales. Again, it is critical to recall this is a median price, not an average.

The 2005 median sales price for new homes in zip code 30005 is most likely an outlier, but it does indicate that the high-end home market is being capitalized upon in this area. It is also clear that the values in 30005 are higher across the board than in 30041. Zip code 30005 is "younger" in terms of significant new residential development; while the home values in the 30005 zip code started higher, they have not had as great of a proportional increase over this same timeframe. However, zip code 30041 has seen significant gains in value over the last five years as existing stock has appreciated and new stock was built at higher prices. Additionally, the sales activity is still volatile in zip code 30005, as developers have been testing the market, this zip code will stabilize markedly in the next few years, as more stock comes online.

EXISTING RESIDENTIAL

Because the area was zoned, and began to develop, as primarily industrial and office uses, residential development located elsewhere in the County. The location of residential in the Study Area is very concentrated. The bulk of residential is found along and off of Stoney Point Road, in the northern portion of the Study Area. However, there is a very interesting collection of types of residential in this area. There are several subdivisions, many with homes in the \$250,000 and up price range, and then there are a few small subdivisions under construction that are in the \$600,000 and up price range. In this same area, there are estates with significant acreage and a single-family

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home and there are also lots with several mobile homes. Additionally, there is at least one subdivision that was constructed a few decades ago, made up of older ranch and split-level homes. The airstrip, such an unusual amenity, is also in this area, surrounded by its own residential base.

Further, there are no examples of multi-family within the Study Area. There are multi-family developments that are immediately adjacent to the Study Area, closest to the southwest portion of the Study Area. There are some within the Windward Parkway office developments, and there are several significant apartment developments just on the other side of GA 400 on Union Hill Road. The rents for these apartments range from \$875 to \$2,000 per month. There are also several townhome developments in this area as well, with price points around \$165,000 to \$180,000. There are some under construction currently, and others were built around 2000.

PLANNED RESIDENTIAL

There is a solid quantity of new and planned development in the Study Area that supports the continued trend of increased housing values. Many developers and property owners indicated that most of the inquiries they deal with are for residential development; they felt there was a pent-up demand for residential in this area. In speaking with local developers, they indicated they are in the area because that is where their customer wants to be. They felt people were moving out of North Fulton, and looking for a comparable alternative with lower taxes. Potential residents like proximity and access to both GA 400 and Lake Lanier. They also have shopping and dining options within a short drive to Johns Creek, Windward Parkway, or North Point.

There are three residential projects under construction within and adjacent to the Study Area. Due to the nature of these developments, as well as their scale, they are expected to have a significant impact on the Study Area, as such they are detailed below.

East Ridge at Stoney Point – Stoney Point Road at Langstrath Lane. Under construction. 50 single-family homes. Prices starting from \$240,000s. Amenities include pool, tennis courts and open space. Second phase of The Ridge at Stoney Point subdivision.

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Stoney Point Farms – *Stoney Point Road at Stoney Point Farm Road*. Under construction. 35 single-family homes. Gated club community. Prices from \$600,000s to \$800,000s. Amenities include pool and tennis courts.

Creekstone Estates – *Stoney Point Road*. Build-out will be 352 homes, 36 under construction currently. Prices from \$600,000s to \$1 million plus. Amenities include: clubhouse with conference, party, exercise, and massage facilities; 11 tennis courts; junior Olympic pool; lake; play field; pro shop and athletic court.

There is a considerable surge in residential development planned in the Study Area. The proposed Ronald Reagan Boulevard is clearly spurring development. There are several new developments that access the Boulevard in some form or fashion. While these developments are not underway and the timelines for phasing are not specific, they are important to note.

	UNITS	TYPE	LOCATION
Fox Creek Properties Development	181 657	Townhomes Apartments (Developer has right to convert all or part of apt. land to SF attached or detached without site plan approval)	GA 400 at Majors Road (bisected by proposed Ronald Reagan Parkway)
Provence Subdivision	347	Single Family Homes	Stoney Point Road (will be additional access via proposed Ronald Reagan Parkway)
Beazer Homes Development	396	Single Family Homes	Stoney Ridge Road (will be additional access via proposed Ronald Reagan Parkway)
Regent Land Holdings Development (mixed use development)	300 TBD	Townhomes Single-Family Homes	McFarland Road at Bluegrass Valley Parkway
WF Orr & Co. Development (mixed use development)	61	Single-Family Homes	Stoney Point Road at Shiloh Road East

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DEMAND ANALYSIS

It was determined that using only new household growth produced from the Study Area itself was the best route for the residential market demand forecast because there is such a high level of residential building activity planned and under review in and near the Study Area, as listed above. Thus, there is a very real possibility that these estimates could ultimately underestimate what happens in this market, similar to the residential activity in other rapidly suburbanizing locations within metro Atlanta that have outpaced expectations the last few years. The focus should realistically be on the next five years, and then the marketplace should be re-assessed since there could be opportunity to capture more growth as this area continues to develop.

ASSUMPTIONS

In order to determine the level of demand for residential product that the Study Area can support, some assumptions had to be made. The addition of 250 households annually was used, based on the combination of forecasts from the Atlanta Regional Commission and Census-based projections. Using only new household growth as a market determination can produce conservative estimates, as demand also comes from turnover within the market. This means there are residents in the Study Area that might move into another location within the Study Area, thus producing a new customer, but not a new household.

Key assumptions were also made about the tenure characteristics and housing preferences. An effort was made to bring them more inline with the consumer preferences shown in areas that are more mature in their residential development process. Since residential is still relatively new to the Study Area, and as development preferences are altered as more and more new people move into the area, it is important to understand their preferences will be a little different than the way it has always been.

For instance, the national average and the Atlanta MSA have renter occupancy rates at roughly 30%. The primary market area reports renter occupancy at about 20%. There will be a natural shift to bring the ratios of owners and renters in closer alignment to greater market area trends as the area becomes more developed. Further, the addition of single-family attached as a viable product option

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is already being seen in the Study Area. Several developments are planned, and already approved, that include sizable townhome components.

Moving forward with assumptions on annual household growth; tenure characteristics (owner versus renter), housing preferences, and residential product trends were then reconciled to produce the final residential demand preferences.

	TENURE PROPORTION		ANNUAL DEMAND		FIVE-YEAR DEMAND		TEN-YEAR DEMAND	
	Owner HH	Renter HH	Owner HH	Renter HH	Owner HH	Renter HH	Owner HH	Renter HH
Single-Family Detached	50%	10%	88	8	438	38	875	75
Single-Family Attached	50%	45%	88	34	438	169	875	338
Apartments	--	45%	--	34	--	169	--	338
Total Units			175	75	875	375	1,750	750
			250		1,250		2,500	

The residential product under construction and planned and approved, as outlined in the prior section, is more than the projected five-year demand. However, there is no clear timeline on the planned and approved projects, and the phasing that might be undertaken. Additionally, these projections are on the conservative side, and it is feasible that the residential growth in the Study Area will be more explosive based on construction of proposed Ronald Reagan Boulevard and developer interest.



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Retail Market Analysis

OVERVIEW

The metro Atlanta retail market suffered from the recession of the 1990s, as did the rest of the nation. It has been making a slow recovery, due in large part to its sprawling boundaries. Given the nature of retail development across such a large metropolis, it is feasible for different submarkets to have completely different and isolated experiences within this recovery period. As a whole, the retail market in Atlanta has a total of 1,915 shopping centers, representing approximately 160 million square feet, with a 10.7% vacancy rate.

Retail sales were hit the hardest as the recession took hold, as fewer consumers were inclined or able to part with their disposable income. But, with added buying power from refinanced mortgages and federal tax cuts, consumers have continued to spend in metro Atlanta, though somewhat more conservatively. During these last few years, retail space in Atlanta has continued to grow. Specialty lifestyle centers are a hot and proven product in Atlanta, with examples like Camp Creek Marketplace and The Forum at Peachtree Parkway. More and more retail space is showing up as components of large mixed-use developments, such as Atlantic Station. Not surprisingly, grocery-anchored retail centers and neighborhood centers continue to be solid products.

The Study Area is located in the Forsyth County/GA 400 North submarket. It has a total of 51 shopping centers, reflecting almost three million square feet of retail space. The vacancy rate is above the metro average, at 12.5%. There is 594,560 square feet currently under construction in this submarket, and another 200,000 square feet planned for development, according to Dorey's.

The Study Area has no retail development of a significant size at this time. It is readily recognizable that people must drive out of the Study Area in order to find goods and services. The retail clusters in the Primary Market Area are located in the Windward Parkway area and Cumming. Residents and employees in the Study Area have to drive for both convenience and destination retail.

There are basically three types of retail functionalities at work in any given market.

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1. Convenience – grocery and drug store purchases, as well as some apparel and home items. Usually purchased close to home, based on available selection. Can also include restaurants.
2. Regional/Chain – more likely to be shoppers goods, such as apparel, home items, hobby-related goods, etc., and restaurants. Consumers travel to specific stores based on the consistency of selection and types of goods. The same consistency and familiarity with product is the driving force behind dining out at chain restaurants as well.
3. Regional/Unique – most likely shoppers goods and restaurants. Consumers will drive long distances to go to stores and restaurants that provide goods and services unlike anywhere else. This uniqueness can be specific products, the environment/atmosphere, or the ability to go to a place that clusters similar goods and services in a hard-to-find fashion.

In short, having all three types of retail functions within the Primary Market Area helps to keep more money in the local economy by meeting all residents' and workers' consumer needs within one area.

EXISTING RETAIL

In the Study Area, there is only a small strip center development near the entrance of Bluegrass Valley Business Park, on McFarland Road near the GA 400 exit. This development is clearly serving business park occupants, as well as convenience retail for those accessing GA 400 here. The retail offerings are gas stations, fast food establishments, a dry cleaner and a small fitness studio. The rest of the Study Area is primarily office and industrial development. As was mentioned in the Residential Market Analysis section, residential development has been small, but is expanding very quickly.

PLANNED RETAIL

There are two mixed-use projects that are planned, and have been approved, within the Study Area, as detailed below.

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Regent Land Holdings Development – McFarland Road at BluegrassValley Parkway. Planned. 167,000 square feet of CBD¹ use. Additional uses include 300 townhomes, and additional single-family homes (number to be determined based on conditioned approval).

WF Orr & Co. Development – Stoney Point Road at Shiloh Road East. Planned. 9,300 square feet of retail space on 1.86 acres. Additional uses include 90,000 square feet of office and 61 single-family homes.

DEMAND ANALYSIS

Demand analysis was conducted in relation to two types of retail development: neighborhood serving and community serving. Neighborhood serving retail usually includes convenience goods and personal services for day-to-day needs of the immediate area. Community serving retail is slightly larger, and provides a wider variety of shops, making merchandise available in a greater array of styles and prices, as well as providing convenience goods and personal services.

ASSUMPTIONS

In order to determine the amount of retail space that the Study Area can support, some assumptions had to be made. Demand analysis used the Study Area for the neighborhood serving retail population base; the Primary Market Area was used for the community serving retail population base, and then the proportion the Study Area could realistically support was determined. The addition of new households computed earlier using the combination of forecasts from the Atlanta Regional Commission and Census-based projections was also utilized here. This growth was then used in calculating supportable retail space by reviewing potential retail sales for the areas and estimating target sales per square feet based on national trends.

¹ CBD use as defined by Forsyth County Unified Development Code: This district is intended to provide for the more intensive commercial uses that serve the weekly and less frequent shopping and service needs of the locality and region. The Commercial Business District zone is intended to allow auto-accommodating commercial development in areas already predominantly built in this manner. The district allows a full range of retail and service businesses serving a local or regional market.

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A total of approximately 539,140 square feet of retail space is supportable in the Study Area currently. It is important to clearly state the maximum supportable is certainly not the exact number of square feet that should be built. The Concept Plan section includes development options and phasing suggestions. Additionally, the community's input was taken into consideration, as well as needed land use and zoning changes.

A breakdown of the components of this total is shown in the table below, as well as projections for five-year demand. Convenience Goods are primarily grocery store and drug store purchases. Shopper Goods are the balance of retail items, such as apparel, home furnishings, hobby-related goods, etc. Food and Beverage is primarily restaurants and liquor stores.

	Convenience Goods		Shoppers Goods		Food & Beverage	
	Existing	Five-Year	Existing	Five-Year	Existing	Five-Year
Neighborhood Serving	44,150	56,110	113,020	143,630	50,940	64,730
Community Serving	68,620	89,920	200,270	262,430	62,140	81,430
Totals	112,770	146,030	313,290	406,060	113,080	146,160

There is sufficient retail demand now to support additional retail space within the Study Area. Since there is not significant retail in the area, most of this demand will have to be new product. However, it is critical to note that this is the maximum determined to be supportable, by no means does that mean it all has to be constructed. In conjunction with the LCI planning process, options on types of development and phasing will be decided. It is important to note supply and demand is not a simple mathematical equation that can be reconciled by considering only the absolute numbers. Instead, it is a study of market dynamics, including customer preferences, quality of retail space, product selection, and location.



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Office Market Analysis

OVERVIEW

The metro Atlanta office market is undergoing a recovery that is long due. Last year brought improvement in terms of net absorption, vacancy and subleases. In the first half of 2005, that trend has continued, with occupancy growth continuing, vacancy rates still declining, and sublease space steadily going down. During second quarter 2005, quoted lease rates seemed to be on the rise for the first time in a year.

The market recovery is certainly more gradual than many past cycles. Some question how accurately a comparison can be made with the record low vacancy rates that occurred five years ago in metro Atlanta as a result of the technology boom. Speculative construction remains relatively limited. However, the brokerage community as a whole seems to believe that we are moving out of recovery mode and cautiously into another expansion mode, according to CB Richard Ellis.

The Study Area is located within the Roswell/Alpharetta/GA 400 submarket for office product. This submarket is located on either side of GA 400 and north of the Chattahoochee River. The Roswell/Alpharetta/GA 400 submarket contains 16.3 million square feet of office space, accounting for 12.6% of the total office space in metro Atlanta. This submarket has strongly rebounded from significant net occupancy losses in 2002, as the technology bubble burst. Vacancies have decreased to its lowest level in two years, as absorption has continued to register positive gains. The current vacancy rate is 26.9%, according to Dorey's.

In an attempt to narrow down this geographically large submarket, statistics were gathered for the North Fulton and Forsyth area² in particular, as this is the primary competition for the Study Area. Characteristics of the market were reviewed for the last two and a half years. There were 427 office properties, which was a 12.6% increase from the end of 2002. Vacancies in office decreased during this time from 22.4% to 16.6%. Rents average \$16.77 per square foot currently. There were 127 flex

² Statistics gathered for 5-mile radius from Alderman Drive and Old Alpharetta Drive, located on northern border of Windward and east of GA 400.

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space properties; there was no change in product quantity during this time frame. Vacancies in flex space average 22.2% and rents average \$7.51 per square foot currently.

The Study Area is predominated by office and industrial development. This has been the primary land use in the Study Area in recent years. The office market has been slow to develop because of an initial lack of infrastructure. As development moved north from the Windward Parkway area in Alpharetta, there was not enough water and sewer in place within the Study Area to support new development, as such, development “skipped over” this area. In the interim, office development continued to thrive in both the Windward Parkway and Johns Creek areas. As a result, the Study Area has more slowly evolved, with primarily business park uses, as opposed to multi-tenant, multi-story office buildings.

Further, the Study Area had some access problems, which have been alleviated with the expansion of the McFarland Road exit expansion at GA 400. Access will continue to improve with the proposed Ronald Reagan Boulevard. In fact, the construction of Ronald Reagan Boulevard is a key component in the future viability of this area in the metro office market. Because of these infrastructure issues, and lack of residential development in close proximity, there have been few amenities in the area. This is part of a location decision when locating office tenants: opportunities for lunch and dinner meetings, lodging for clients, as well as convenience goods and services for employees.

EXISTING OFFICE

The office uses that are located in the Study Area are in three areas.

Windward North. The southwest portion of the Study Area is a beneficiary of its proximity to the northern-most portion of the Windward Concourse development. This area has some higher-end space that is multi-story and multi-tenant buildings. There are some individual industrial uses in this area as well.

McFarland/McGinnis Ferry. There are a few large buildings that were build-to-suit uses for large office/industrial tenants. These are primarily single buildings, though they sit on land that is within the larger Bluegrass development.

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The Meadows at Bluegrass. There is significant business park development along Shiloh Road, along with some being accessible via McFarland Road. This development is predominately single-story buildings with large footprints. There seems to be a solid mix of types of industries and business sectors located within the area. There seems to be a diversity of uses as well, with distribution, research, warehousing, and office space all represented in the area.

PLANNED OFFICE

Speculative office space is now under construction and planned both within the Study Area and directly adjacent.

Corporate Point at the Meadows. Three buildings under construction. Total of 148,318 square feet of office space. One three-story building at 35,960 square feet; one two-story building at 80,758 square feet; and one two-story building at 31,600 square feet.

Corporate Pointe Business Park. Under construction. 230,000 square feet of office and warehouse condo space. Three buildings total, two as four-story and one as three-story. Amenities to include: 24-hour full gym, coffee shop, gourmet café and access to nature preserve and walking trail.

Nichols & WF Orr Development. Planned. 224,800 square feet of office building and office warehouse space. Planned in four phases. Total of 15 buildings at build-out. Buildings will range from 6,000 square feet to 30,000 square feet in size.

There are two mixed-use projects that are planned, and have been approved, within the Study Area, as detailed below.

Regent Land Holdings Development – McFarland Road at Bluegrass Valley Parkway. Planned. 167,000 square feet of CBD³ use. Additional uses include 300 townhomes, and additional single-family homes (number to be determined based on conditioned approval).

³ CBD use as defined by Forsyth County Unified Development Code: This district is intended to provide for the more intensive commercial uses that serve the weekly and less frequent shopping and service needs of the locality and region. The Commercial Business District zone is intended to allow auto-accommodating commercial development in areas already predominantly built in this manner. The district allows a full range of retail and service businesses serving a local or regional market.

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WF Orr & Co. Development – Stoney Point Road at Shiloh Road East. Planned. 90,000 square feet of office on 11.71 acres. Additional uses include 9,300 square feet of retail space and 61 single-family homes.

DEMAND ANALYSIS

In order to determine the amount of office uses that the Study Area can support, some assumptions had to be made.

ASSUMPTIONS

The office demand analysis was conducted on the Primary Market Area and then the capture rate of the Study Area was determined. The addition of new households computed earlier using the combination of forecasts from the Atlanta Regional Commission and Census-based projections was also utilized here, with an assumption that office employment has a ratio of about 0.020 to total population, which is based on national averages. Further, office employment was then translated to square footages based on a ratio of 250 square feet to each employee, again based on national averages. Finally, a capture rate of the Primary Market Area was determined to be 45%.

Existing Demand	Five-Year Demand	Ten-Year Demand
165,875 SF	932,350 SF	2,173,600 SF

The office product under construction and planned and approved, as outlined in the prior section, is almost as much as the projected five-year demand. However, for office demand, it is important to highlight that this product base is serving a greater area than Forsyth County. As development in the Study Area continues, and the residential base increases, the demand for office space will most likely continue to increase over this timeframe based on the income and educational levels of the residents in the area.

Interestingly, office space is actually one of the most difficult land uses to recruit. Developers stick to stringent requirements for access, amenities, location and agglomeration. This basically means that

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office begets office; office is a use that most often clusters together. As alluded to earlier, the trend sequence is usually that residential helps to lead to retail that in turn helps to attract office.

Industrial Market Analysis

OVERVIEW

Much like the office market, the industrial market in metro Atlanta has been making a slow recovery over the last few years, inching towards its peak seen during the 1990s. Last year continued that positive trend, and demonstrated the strongest industrial performance in metro Atlanta in four years. Net absorption continues to be positive, vacancy rates are at their lowest point in three years, and rental rates seem to be evening out at a rate slightly higher than recent quarters. The only point of concern starting to rise in the first half of 2005 is space under construction, both those underway and projects that are beginning. Continued significant additions to supply could slow the growth performance that the metro area is currently enjoying.

As is well-known, Atlanta has many characteristics that have made it the southeastern hub for transportation, distribution and logistics, such as interstate highways, rail lines, and the airport. For all these reasons, metro Atlanta is still a strong location choice for industry. The industrial market is expected to continue to experience a strong recovery, but growth will be moderate unless higher job growth rates are seen or more companies relocate to the area. As those trends level out, many industry insiders caution against continued speculative space development, according to CB Richard Ellis.

The Study Area is in the Central Perimeter (I-285/GA 400) submarket. This submarket runs from I-285, following GA 400, up through North Fulton and Forsyth County. Typically, office development in this area had been in the form of office campuses, and controlled build-to-suit environments. That changed in the 1990s, as the area became more recognizable with the development of North Point Mall, much more office product has been developed, with a greater range in product type. Land costs are rising rapidly in the area, making speculative development more risky than in other industrial submarkets. Additionally, industrial condos are a growing trend nationwide, and might

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make a particularly good fit in this area. Much of the nearly 250,000 square feet currently under construction in the submarket is within the Study Area, and another 175,000 square feet is planned for development in or near the Study Area, according to Dorey's.

In an attempt to narrow down this geographically large submarket, statistics were gathered for the North Fulton and Forsyth area⁴ in particular, as this is the primary competition for the Study Area. Characteristics of the market were reviewed for the last two and a half years. There were 79 industrial properties; there was no change in product quantity during this time frame. Vacancies in industrial average 16.4% and rents average \$7.00 per square foot currently. There were 127 flex space properties; there was no change in product quantity during this time frame. Vacancies in flex space average 22.2% and rents average \$7.51 per square foot currently.

As mentioned in the preceding section, Office Market Analysis, much of the development in the Study Area has been office and industrial in years past. Industrial certainly led the initial development, but the movement within the Study Area has been towards more business park formats.

EXISTING INDUSTRIAL

There are no active industrial uses in the Study Area that fit into the image some people hold of dirty factories, large parcels of inefficiently used land, pollution from manufacturing or old run-down buildings. The industrial within the Study Area is clean industry and primarily light industrial. There are distribution and warehouse uses, as well as service space. Because of its nature, it can fit easily into a business park, next to buildings that house professional services. Due to this characterization of both office campuses and built-to-suit buildings, there is not really a need to separate industrial and office markets for the Study Area.

PLANNED INDUSTRIAL

Speculative business park space is now under construction within the Study Area.

⁴ Statistics gathered for 5-mile radius from Alderman Drive and Old Alpharetta Drive, located on northern border of Windward and east of GA 400.

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Bluegrass Commons Industrial Park. Planned. Distribution facility with 20% office space. Total of 105,400 square feet.

DEMAND ANALYSIS

As stated above, the office and industrial markets so closely mirror each other and overlap for the Study Area based on product type, it is not necessary to produce separate analyses. Please refer to Office Market Analysis section for more detail on demand characteristics.





Section B. Development Plan

Community Participation

The Community Development Plan is the result of the efforts facilitated by the team of consultants. This component of the LCI Plan involved community participation. Invitations to participate were extended to a variety of stakeholders, including:

- Study Area residents ;
- Study Area Property Owners ;
- Forsyth County Elected Officials ;
- Forsyth County Staff, particularly planners and engineers ; and
- Regional Transportation Interests (GRTA, ARC, GDOT, MARTA)

The following mechanisms were utilized in an attempt to attract these various stakeholders to participate.

Core Committee. All goals, issues, findings, and recommendations were presented to and discussed with the Core Committee at regular monthly meetings.

Stakeholder Interviews. The County identified a list of key stakeholders including area residents, property owners, and developers. Representatives of these groups were interviewed in order to ascertain their stake in the future of the Study Area.

Community Preference Survey (CPS). The CPS offered the opportunity for the community to express preferences for several aspects of urban design and transportation facilities.

Design Workshop. A major component of the design workshops was community input on how best to “connect” the proposed facilities and land uses. Important concepts as well as specific project ideas evolved from the community involvement. These were examined for economic, engineering, and political feasibility by the consulting team and incorporated in the master plan.

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Concept Plan. The consulting team assimilated the findings and interests and prepared a concept plan which addresses land use, urban design, and transportation. Planners attempted to balance the community vision with political, financial, engineering, and market feasibility.

Transportation Coordination Meeting (TCM). Because many transportation projects are affected by organizations and agencies outside of local jurisdiction, a TCM was held to discuss potential projects prior to finalizing recommendations. GMRDC, ARC, GDOT, and Forsyth County BOE as well as Forsyth County planning and engineering departments were represented. The attendants determined that the final transportation recommendations which follow in Section C are feasible.



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Character Preference Results

A character preference study was conducted by the Sizemore Group to allow participants of the McFarland-Stoney Point LCI Study to express their preference for the visual image of their community. The extensive list of images was selected by the Sizemore Group to provide a wide range of choices. A summary of the selections is presented below.

Parks and Gathering Spaces

In the category of parks and gathering places, the preferred choice of image reflects a space that should be designed to encourage social and passive recreational activities. The importance of interconnected spaces and the ability to encourage a variety of social opportunities is reflected. The choice also expresses a desire for neighborhood-scale parks and gathering spaces versus community or regional parks. Large civic gathering spaces like pavilions and town greens received a neutral rating.

<p>McFarland-Union Hill-Stoney Point LCI Category #1</p> <p>Parks and Gathering Places</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Variety of social opportunities + Encourages interaction + Natural Landscape qualities <p>B</p>		<p>RATING</p> <p>DESIRE</p>
	<ul style="list-style-type: none"> + Provides social opportunity + Opportunities for active recreation <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>
	<ul style="list-style-type: none"> + Recreation opportunities + Provides social opportunity <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>



A-
Public open spaces of all scales encourage social, civic, and recreational activities. All such spaces should be designed to encourage these activities and should be interconnected to provide a variety of experiences.



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Greenways and Trails

These should provide a variety of pedestrian and biking experiences through the natural and built environment. These spaces should have periodic shade opportunities and sufficient open spaces to comfort and enhance the perception of safety. The image promotes visual and physical buffers between pedestrians and traffic are encouraged.

At the same time other images that reflect trails with tunnels providing below grade road crossing, where safety may be a concern, received only a neutral rating.

<p>McFarland-Union Hill-Stoney Point LCI Category #2 Greenways & Trails</p>	<ul style="list-style-type: none"> + Natural features + Removed from traffic + Park like setting with natural areas around <p>B+</p>		<p>RATING</p> <p>DESIRABLE</p>
<p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Walking/jogging opportunity + Variety / anticipation - Asphalt trail <p>B</p>		<p>RATING</p> <p>DESIRABLE</p>
<p>Greenways & Trails should provide a variety of pedestrian, biking experiences through the natural and built environment. Periodic shade opportunities and sufficient open spaces contribute to comfort and perception of safety. Visual and physical buffers between pedestrians and traffic are encouraged.</p> <p>A-</p>	<ul style="list-style-type: none"> + Walking/jogging opportunity - May not feel safe - Narrow pathway feel <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>



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Sidewalks/Streetscapes Urban

The image reflects the communities' desires for providing sufficient pedestrian circulation without overcrowding. Sidewalk furniture, overhangs from buildings, awnings and green medians contribute to a positive urban experience. The need for physical and visual buffers between pedestrians and traffic are desirable.

Images where there are large interruptions in street façade image and inadequate pedestrian provisions received neutral ratings. The respondents found images where there was no separation from traffic, poor visual attraction and felt unsafe as undesirable.

<p>McFarland-Union Hill-Stoney Point LCI Category #3 Sidewalks/Streetscape - Urban</p>	<ul style="list-style-type: none"> + Canopies and tree cover + Separation from traffic + Wide enough for circulation 		<p>RATING</p> <p>DESIRABLE</p>
<p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Planters/lighting fixtures at street edge + Trees and parking at street edge as buffers - Articulation of pedestrian crossing 		<p>RATING</p> <p>NEUTRAL</p>
<p>B</p> <p>Walks and walkways should provide for sufficient pedestrian circulation without overcrowding. Overhangs, trees, textured paving, street furniture, chairs & tables, green medians -- all contribute to a positive experience. Visual and physical buffers between pedestrians and traffic are encouraged.</p>	<ul style="list-style-type: none"> - No separation from traffic - Feels unsafe and dangerous - Visually not attractive - Exposed utilities 		<p>RATING</p> <p>UNDESIRABLE</p>



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FORSYTH FORWARD



Sidewalks/Streetscapes Suburban/Residential

The preferred image reflects the communities' desires for shaded walkways and green median boulevards to help establish neighborhood scale and a visual appeal of the streets/neighborhood. Overhangs, trees, green medians -- all contribute to a positive experience. Visual and physical buffers between pedestrians and traffic are encouraged.

Images where there was a lack of vegetation and landscaping received neutral ratings along with images overhead utilities and narrow sidewalks.

<p>McFarland-Union Hill-Stoney Point LCI Category #4</p> <p>Sidewalks/Streetscapes-Suburban/Resident</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Tree canopy + Narrow roadway + Green strip between the street and the sidewalk - buffers <p>B</p>		<p>RATING</p> <p>DESIRABLE</p>
	<ul style="list-style-type: none"> + Defined sidewalks and green strip - Sidewalk feels "too close" to private space - Lacks vegetation - trees - Narrow sidewalks <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>
<p>Shaded walkways and green median boulevards help the neighborhood scale and visual appeal of the streets/neighborhood. Overhangs, trees, green medians -- all contribute to a positive experience. Visual and physical buffers between pedestrians and traffic are encouraged.</p> <p>A-</p>	<ul style="list-style-type: none"> + Stone wall defines the public and private space - Prominent overhead utilities - No street trees <p>C-</p>		<p>RATING</p> <p>NEUTRAL</p>



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FORSYTH FORWARD



Storefronts/Character Design – Single Use

The example of high quality in this category reflects a character that provides variety to the pedestrian experience. Seating, landscaping and awnings that reinforce the human scale all contribute to the shopping experience. Parallel parking is encouraged.

The survey shows that storefronts that were dominated by automobiles, with no trees, a lack of pedestrian scale and little or no storefront design interest received an undesirable rating.

<p>McFarland-Union Hill-Stoney Point LCI Category #5</p> <p>Storefronts Design Character-Single Use</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Good architecture/building form + Pedestrian scale + Interesting storefront variety and streetscape <p>B</p>		<p>RATING</p> <p>DESIRABLE</p>
	<ul style="list-style-type: none"> + Variety of building form - Bland / residential style - Narrow sidewalk - Cars encroach on walkway <p>C-</p>		<p>RATING</p> <p>NEUTRAL</p>
<p>Storefronts provide a critical element of visual variety to the pedestrian experience. Parallel parking is encouraged. Seating, landscaping, canopies – all contribute to the shopping experience. Storefronts should reflect the human scale.</p> <p>A-</p>	<ul style="list-style-type: none"> - Automobile dominated - No trees - Single story commercial buildings - No storefront design interest <p>D-</p>		<p>RATING</p> <p>SOMEWHAT UNDESIRABLE</p>



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FORSYTH FORWARD



Storefronts/Character Design – Mixed-Use

In line with the results of the previous category, the example of high quality in the mixed-use category reflects a character that provides variety to the pedestrian experience with common elements that tie the environment together. Seating, landscaping and awnings that reinforce the human scale all contribute to the experience. In terms of building height within the mixed-use environment, mid-rise building of three to five stories are encouraged.

The survey shows that facades that are dominated by blank walls and a lack of fenestration are undesirable.

<p>McFarland-Union Hill-Stoney Point LCI Category #6 Storefronts/Design Character-Mixed-Use</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Building edge, canopies, streetscape + Continuous street façade with roof variety + Pedestrian friendly (scale) street <p>B-</p>		<p>RATING</p> <p>SOMEWHAT DESIRABLE</p>
	<ul style="list-style-type: none"> + Textured pavement and street furniture + Building edge & canopies - Lacks trees/streetscape - Lack of appealing signage <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>
<p>Storefronts provide a critical element of visual variety to the pedestrian experience. Parallel parking is encouraged. Seating, street furniture, building edges, landscaping, canopies -- all contribute to the shopping experience. Storefronts should reflect the human scale.</p> <p>B</p>	<ul style="list-style-type: none"> - Warehouse feel, large building mass, has no architectural interest - Isolated feel surrounded by parking - Lacks pedestrian scale <p>D-</p>		<p>RATING</p> <p>UNDESIRABLE</p>



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FORSYTH FORWARD



Residential – Multi-Family:

Many of the images in the multi-family category did not score highly. This indicates either the dissatisfaction with the multi-family palette shown, or the desire to maintain a single-family character. The images that received the highest ratings within this category reflect details such as balconies, historic detailing, rhythmic fenestration, quality materials and a street presence.

Large apartment buildings with unattractive architecture, awkward detailing features and large parking lots in front of buildings received responses that ranged from undesirable to unacceptable.

<p>McFarland-Union Hill-Stoney Point LCI Category #7 Residential Multi-family</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Street presence + Too many driveways - Stark streetscape <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>
<p>Multi-family residential units located directly on the street are critical components in Smart Growth planning -- often located above commercial retail storefronts. Balconies, historic detailing, rhythmic fenestration and quality materials contribute to this component.</p> <p>C+</p>	<ul style="list-style-type: none"> - Large building mass – crowded feel - Not pedestrian friendly - Lacks streetscape <p>D</p>		<p>RATING</p> <p>UNDESIRABLE</p>
<p>C+</p>	<ul style="list-style-type: none"> - Commercial feel - architecture - Lacks Streetscape <p>D-</p>		<p>RATING</p> <p>SOMEWHAT UNDESIRABLE</p>



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Residential – Single-family:

The images that scored highly in this category reflect quality detailing, historic detailing and materials, front porches, buffer yards, and shade trees. In addition, homes that address the street with garages either detached or not visible from the sidewalk scored well.

Homes with large garages and driveways that dominate the façade scored poorly along with homes that do not reflect quality and detailing.

<p>McFarland-Union Hill-Stoney Point LCI Category #8</p> <p>Residential Single-family</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Detailing and material diversity + Clean look and well maintained + Front porch + Landscaping <p>B</p>		<p>RATING</p> <p>DESIRE</p>
	<ul style="list-style-type: none"> + Garage set back - not part of façade + Richness in material - Driveway in front <p>B-</p>		<p>RATING</p> <p>SOMEWHAT DESIRE</p>
<p>Single-family residential units should reflect quality detailing, historic detailing and materials, front porches, buffer yards, and shade trees.</p> <p>B+</p>	<ul style="list-style-type: none"> + Architectural details/elements - Garage prominent in façade - No front yard - Lack of Street trees <p>C+</p>		<p>RATING</p> <p>NEUTRAL</p>



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FORSYTH FORWARD



Civic – Public Buildings:

Civic and public buildings play a strong role in providing a visible symbol for the community and could serve as a catalyst for positive development. The examples of high quality preferences in this category were strong anchors on the street and had good vertical massing. The presence of historic detailing and quality materials was also reflected in the choice.

Images that had no street presence, were incongruous in style and lacked landscaping were deemed undesirable for the LCI Study Area.

<p>McFarland-Union Hill-Stoney Point LCI Category #9</p> <p>Civic/Public Buildings</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Traditional style + Good visual composition/details + Good architecture and massing <p>B</p>		<p>RATING</p> <p>DESIREABLE</p>
	<ul style="list-style-type: none"> + Arched articulation – solids and voids - Lacks cohesive landscape - Large mass <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>
<p>Civic structures play an important role to galvanize a community and lend a visible symbol of commonality. They also provide a critical functional role in the development of a live-work-play environment, and can be a catalyst for other positive development.</p>	<p>B+</p> <ul style="list-style-type: none"> - Too busy - No landscaping - Inappropriate design <p>D</p>		<p>RATING</p> <p>SOMEWHAT UNDESIREABLE</p>



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Parking:

No single image of parking presented in this survey received an overwhelming support from the community. Instead participants selected both well landscaped parking lots and well designed decks as desirable images. The desirable images in this category had vehicles in clusters and surrounded by visual screening and landscaping. Parking decks with screened facades and well lit were also desirable to the community.

Large parking lots with no landscaping and visual screens were undesirable.

<p>McFarland-Union Hill-Stoney Point LCI Category #10 Parking</p> <p>HIGH QUALITY CATEGORY</p>	<ul style="list-style-type: none"> + Trees as shade/buffers - Large asphalt area <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>
	<ul style="list-style-type: none"> + Parking as buffers to sidewalks/pedestrians - Lacks landscape <p>D</p>		<p>RATING</p> <p>NEUTRAL</p>
<p>Parking, though necessary, should not drive the master plan. Vehicles should be clustered, and surrounded by landscaping. Visual screening is desirable along streets - either landscaped, walled or fenced.</p> <p>B</p>	<ul style="list-style-type: none"> - Sea of parked cars - No landscaping - Building is barely visible <p>F</p>		<p>RATING</p> <p>UNDESIRABLE</p>



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Signage/Artwork/Street Furniture:

The image of high quality in this category reflects a community where hanging signs and signs that are integrated into canopies are encouraged. The signage should be of appropriate scale and detailing.

Free standing signs that are a visual clutter, with an 'offensive' scale and lacking design interest were undesirable.

<p>McFarland-Union Hill-Stoney Point LCI Category #11</p> <p>Signage/Artwork/ Street Furniture</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Good landscape details + Paving/streetscape /trees/banners <p>B</p>		<p>RATING</p> <p>DESIRABLE</p>
	<ul style="list-style-type: none"> + Signage integrated in building canopy design - Lacks streetscape/buffers to the street <p>C</p>		<p>RATING</p> <p>NEUTRAL</p>
<p>Street furniture, lighting, etc. enhance the quality and character of the environment. Hanging signs and signs integrated into canopies are encouraged. Freestanding signs should have quality detailing. Large scale mall signage is absolutely discouraged.</p> <p>B+</p>	<ul style="list-style-type: none"> - Visual clutter - Unattractive - Offensive scale - Lacking quality detailing & consistent type set <p>F</p>		<p>RATING</p> <p>UNDESIRABLE</p>



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Traffic Calming:

Traffic calming measures should also enhance the visual elements for the community. Textured pavements were rated desirable; traffic circles were scored as neutral.

Simple economical solutions like median dividers that don't contribute to street character were undesirable.

<p>McFarland-Union Hill-Stoney Point LCI Category #12 Traffic Calming</p> <p>EXAMPLE OF HIGH QUALITY IN THIS CATEGORY</p>	<ul style="list-style-type: none"> + Defines the intersection + Alerts the motorists to the intersection + Helps pedestrian crossing <p style="text-align: center;">C</p>		<p style="text-align: center;">RATING</p> <p style="text-align: center;">NEUTRAL</p>
			<p style="text-align: center;">RATING</p>
<p>Textured Pavement</p> <p>Traffic calming mechanisms should contribute to the style and character of the community in addition to effectively calming driver behavior.</p> <p style="text-align: center;">B</p>			<p style="text-align: center;">RATING</p>



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Public Design Charette and Results

Once the Study Area analysis, stakeholder interviews and questionnaires were completed and synthesized, issues were identified and the stage was set for the public design charette - a forum in which the development, open space, parks, safety, connectivity issues, concerns and desires of the community were addressed. The event was held on two consecutive Monday evenings, September 12 and September 19, 2005 at South Forsyth Middle School between 6:00 PM and 8:00 PM.

At the start of the charette, the community was briefed on the goals, issues and needs that were identified through the community visioning process. The participants were then given a briefing on the charette process and outline.

The intent of the design charette was to solicit community input in the design of the LCI Study Area. The charette format was designed as a hands-on design process involving the community, wherein the participants were organized in groups and assisted by professionals from the consultant team. The two charette sessions focused on two different geographic scales: the first night took into consideration the entire Study Area while the second night addressed previously identified development nodes in greater detail.

The plan options were generated through a process of investigating the Study Area and addressing the following questions in the following order:

1. *What would you preserve?*
2. *What would you change?*
3. *What would you create?*
4. *What would you connect?*

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The graphic results of these questions were recorded on transparent overlays on top of a base map of the Study Area. Once the overlays were created, master plan options began to emerge; the plans were then critiqued by each team and then further refined. At the end of the design workshop the refined plans were presented by a designated community member within each team. The pros and cons of each teams' plan were discussed.

The following section presents a consolidated summary of the design workshop and additional input from the core team members.

What would you like to Preserve?

When the community was asked what they want to preserve within the Study Area, the following physical elements were identified:

- Floodplains
- Stream beds/creeks
- Forsyth Primitive Baptist Church and Cemetery
- Union Hill Baptist Church and Cemetery
- Lanier Tech
- Kitfox Airstrip and associated residential
- "The Ridge at Stoney Point" subdivision
- Tree canopy and median along Shiloh Road
- The new retail along McFarland Road at the GA 400 Node
- Portions of green space within all major undeveloped areas within the Study Area



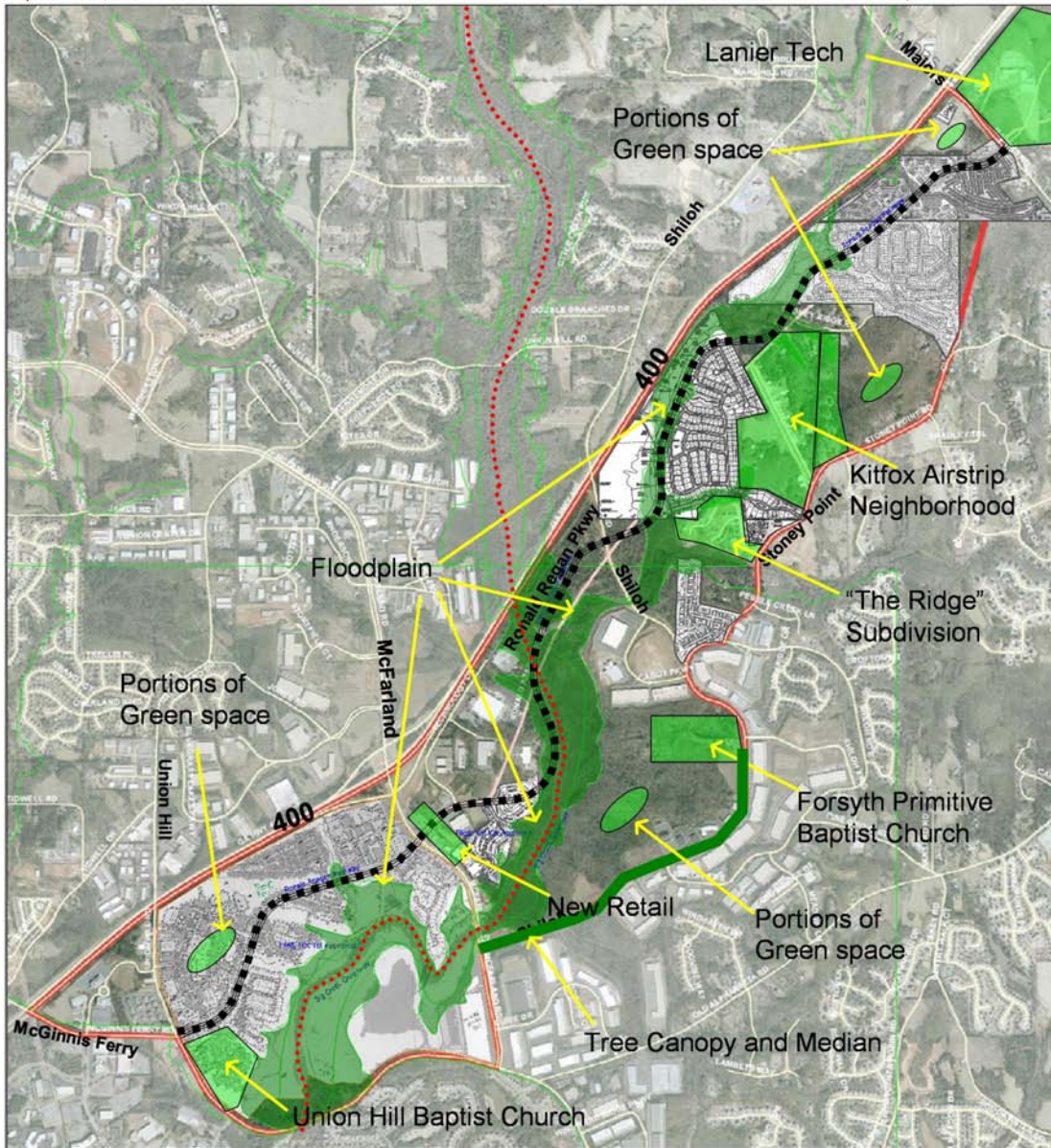
McFarland - Stoney Point Livable Center Initiative

FORSYTH FORWARD



September 12, 2005

MCFARLAND-UNION HILL-STONEY POINT LCI, FORSYTH COUNTY



Consolidated Workshop Results

Preserve



McFarland - Stoney Point Livable Center Initiative

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What would you like to Change?

Due to the fact that the majority of the LCI Study Area is either green field or recently developed, there were not many physical elements that the community identified for change. The few existing elements that were identified for change include:

- The abandoned church at Shiloh Road and Stoney Point.
- The big-box, vacant character of the Cabot Business Park.
- The blind spot at the entrance to 'The Ridge at Stoney Point subdivision on Stoney Point Road.



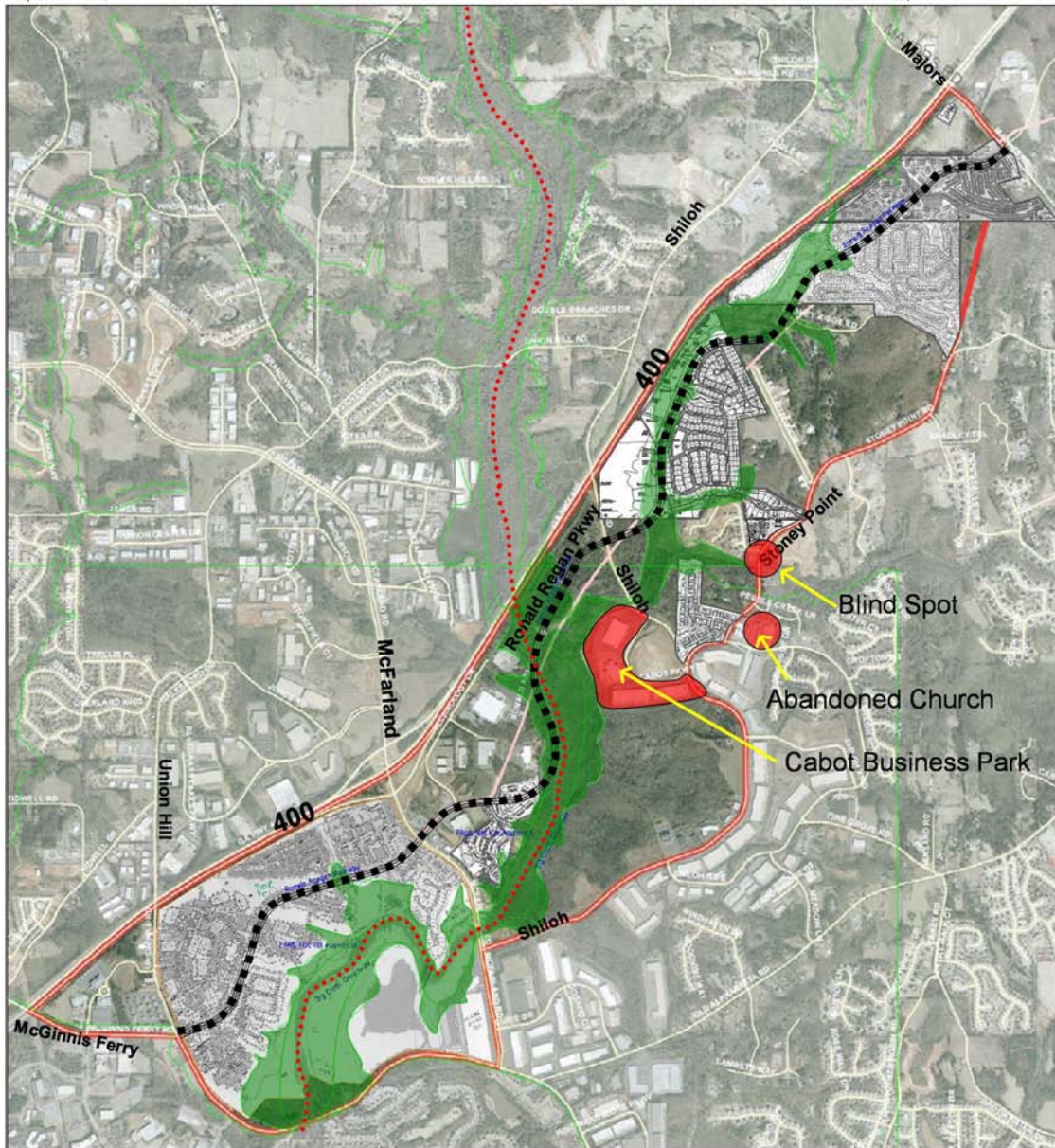
McFarland - Stoney Point Livable Center Initiative

FORSYTH FORWARD



September 12, 2005

MCFARLAND-UNION HILL-STONEY POINT LCI, FORSYTH COUNTY



Consolidated Workshop Results

Change



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FORSYTH FORWARD



What would you like to Create/Connect?

Despite the diversity and range of interests that were present during the workshop, most community members and professionals agreed when it came to the question of what should be created and connected within the Study Area. In summary, the physical improvements that were identified include:

- Opportunities for the expansion of Lanier Tech and the potential to introduce medical training related support functions.
- A landscaped acoustical barrier along the entire length of GA 400, within the Study Area.
- A senior village with a mix of residential options and neighborhood-scale support functions at the undeveloped node west of the Kitfox Airstrip.
- Road improvements to slow down traffic and improve visibility on Stoney Point Road within the vicinity of 'The Ridge at Stoney Point' subdivision.
- The redevelopment of Cabot Business Park into an active/passive community park with the potential for the incorporation of a new school, or other civic functions.
- A well-connected community scale mixed-use center with retail, office and a heavy residential focus at the undeveloped node north of Shiloh Road, just west of the McFarland Road intersection and southwest of the Primitive Baptist Church.
- The potential for a MARTA Park-N-Ride behind the existing retail on McFarland Road at the GA 400 Node.
- A well connected high density mixed-use center west of Union Hill at the GA 400 Node with a Conference Center, Cultural Center, retail, residential and office space.
- Multi-Purpose greenway trail extension to Lanier Tech from the main Big Creek Trail.
- Additional east-west connectivity between the proposed Ronald Reagan Boulevard and Shiloh Road/Stoney Point Road.
- Sidewalks and bike lanes along Stoney Point Road.
- Connector between Technology Drive and the proposed Ronald Reagan Boulevard.
- Traffic calming measures within proposed and existing development nodes.
- The community also wants to ensure that open spaces and parks be created when developing existing green field land.

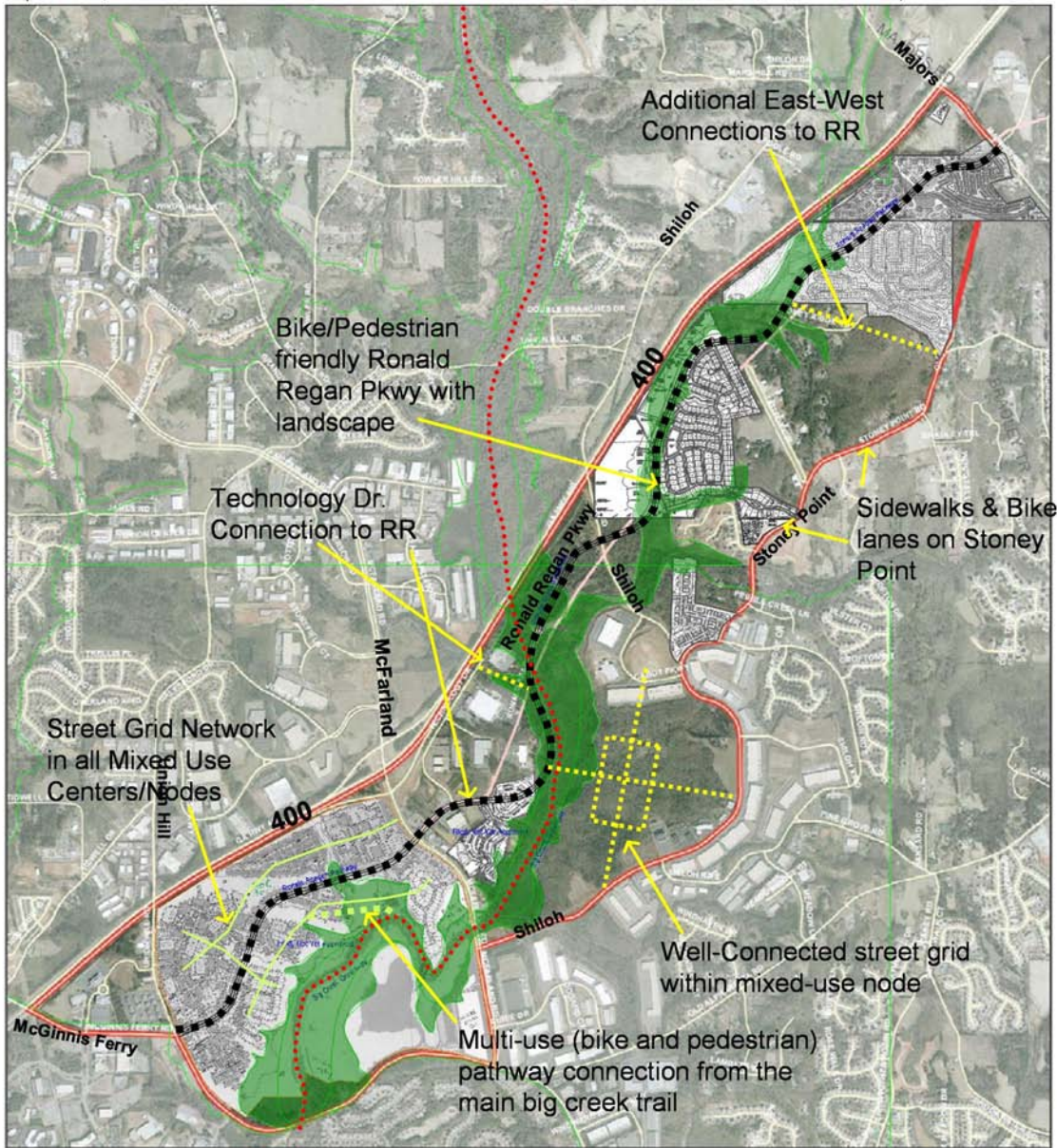
McFarland - Stoney Point Livable Center Initiative

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September 12, 2005

MCFARLAND-UNION HILL-STONEY POINT LCI, FORSYTH COUNTY



Consolidated Workshop Results

Connect



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Concept Plan

The concept plan for the Study Area is developed based on the vision and goals of the community: residents, businesses, property owners, stakeholders, county and other civic, religious and social institutions. The concept plan incorporates 12 major initiatives. These initiatives are designed to leverage the resources of the Study Area and promote a variety of land use and activities and create a pedestrian friendly environment. These initiatives diverge from the current mode of development, but they are designed to promote a vibrant community with unique identity, sense of place and one that is sustainable. This would be a new direction/approach to development in Forsyth County which is experiencing rapid growth trends and development pressures. This model could be used for other areas of the County with similar goals and aspirations.

A significant portion of the Study Area is presently vacant land with a few horse farms with retail along McFarland Road and office/warehouse along a stretch of Shiloh Road from McFarland Road to Stoney Point Road. There are larger homes and estates in the northern section of the Study Area. The residents relate to the image of countryside with quaint architectural features and desire to preserve the estate homes that are characteristic of the Study Area. They would like to see this image reflected in the design elements that can render unique identity to the Study Area.

There are a number of housing developments in the northern end of the site that are zoned and approved and slated to begin construction shortly. These are traditional type of subdivisions that are gated or enclosed and inward looking with cul-de-sac development with single type/use. Most of the other vacant parcels are also in the process of gaining approvals for development. If all of the approved plans and the proposed plans go through their construction, it would build out the entire Study Area. We are at the right juncture to ensure that these green fields transform into a community that is sustainable. A community that offers good street network, alternative mode of transportation, pedestrian friendly environment, promotes social and cultural interaction and enriches the quality of life. The intent is to diverge from the suburban sprawl type of development pattern to cohesive interconnected pattern of development with different uses mixed with pockets of urban mixed-use villages and interspersed with nodes of activities and green space system of parks and trails.

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The following are the community-identified objectives that guided the concept plan.

- To preserve Single-family homes and estates and unique buildings
- To provide diverse housing types and product mix that promotes economic, social and cultural mix
- To preserve the Big Creek buffer and provide greenway (trail) linkage to communities along the creek
- To design Ronald Reagan Boulevard as a pedestrian- and bicycle-friendly boulevard and not merely as an arterial meant to relieve traffic on GA 400
- To provide a good hierarchical street network that offers alternative ways for traffic circulation
- To provide bike trails and other multi-use trails
- To create a pedestrian friendly/scale streets with good sidewalks and streetscape
- To create a unique destination oriented mixed-use village with restaurants, boutique shops, hotel/conference, office and housing.
- To capture the potential office market that can attract more residents and also offer choice for live and work in close proximity and create a strong economic base with jobs and tax revenues to the County.
- To leverage the presence of Northside Hospital and the Lanier Tech in close proximity to promote offices/business related to these institutions
- To provide good amenities, such as parks, playgrounds, potential civic facilities such as school, library, fire station and others in the future.
- To provide small parks and pocket parks throughout the area in close proximity for the residents (within a 5-minute walking radius)

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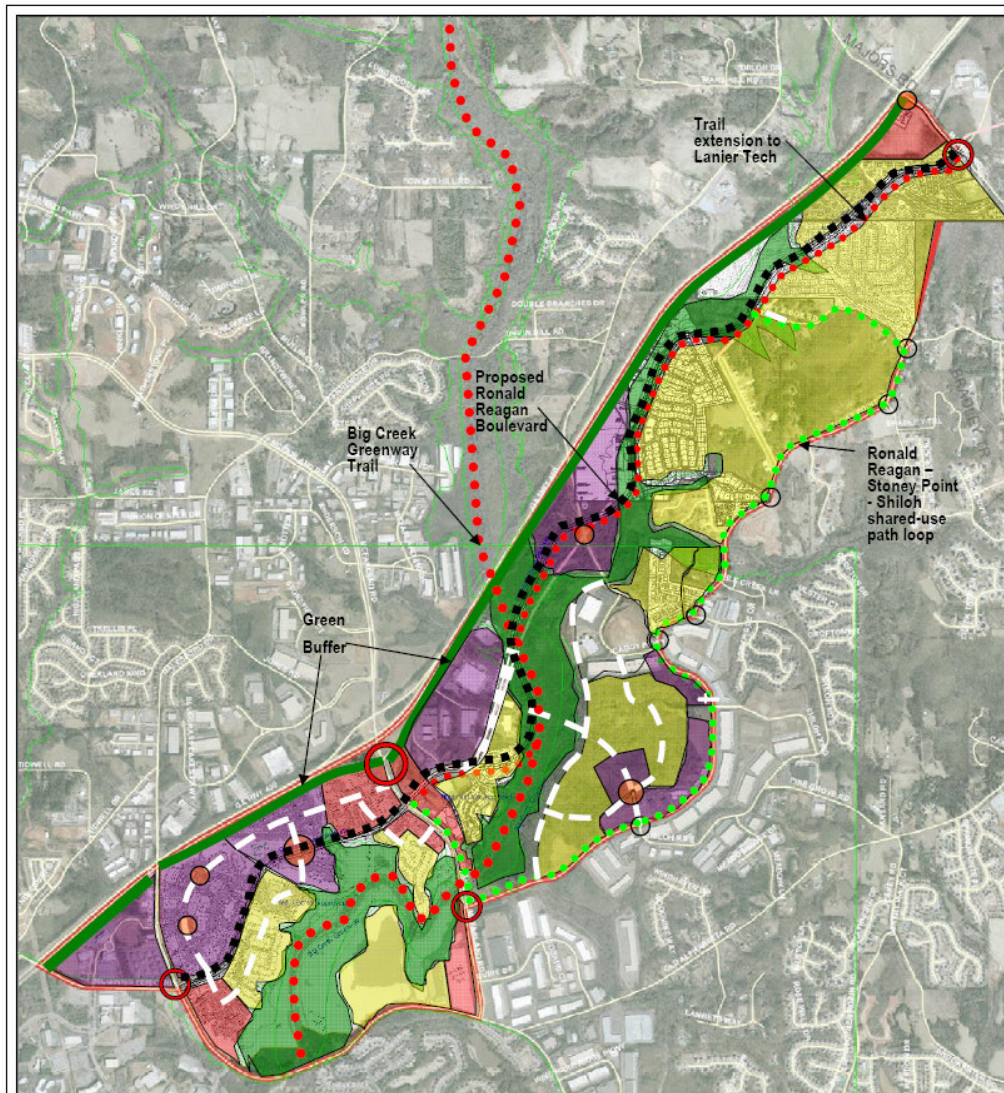
The overall concept plan presented on the following page represents a compilation of the public workshop efforts with additional consideration of input from county representatives and planning professionals.

In general, the high intensity uses and activities are concentrated along the GA 400 exit/McFarland Corridor with lesser intense uses and activities as you move toward the northern part of the Study Area. The proposed Ronald Reagan Boulevard that runs across the Study Area from north to south along GA 400 provides connectivity across the Study Area. The Big Creek floodplain buffer provides the opportunity for greenway connectivity as well. The current Big Creek Trail that is underway sets the tone for the first leg of this initiative, a new multi-purpose trail is proposed that spurs from this Big Creek Trail extending up to the northern end of the Study Area culminating at the Lanier Tech providing connectivity to the Lanier Tech campus. This provides opportunity for connectivity to developments along this corridor on both sides of the Big Creek buffer. The much anticipated Ronald Reagan Boulevard is envisioned not as a mere relief roadway to GA 400 traffic as a traffic mover or bypass to GA 400 but as a boulevard that takes different forms and character as it embraces different uses and development along the boulevard.



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Development Plan Map 2 of 2
Final Concept Plan

- | | | | |
|-------------|----------------|--------------------|-----------------|
| Retail | Mixed Use | Proposed Boulevard | Node |
| Residential | Floodplain | Multi-Use Trail | Traffic Calming |
| Office | Community Park | Vegetation Buffer | Gateway |

McFarland - Union Hill LCI

Forsyth County, Georgia

Scale: 0 2500'



McFarland - Stoney Point Livable Center Initiative

FORSYTH FORWARD



Development Areas

The Study Area is divided into subareas that provide a variety of opportunities to create a vibrant and sustainable community with good mix of land uses and activities. The development areas are:

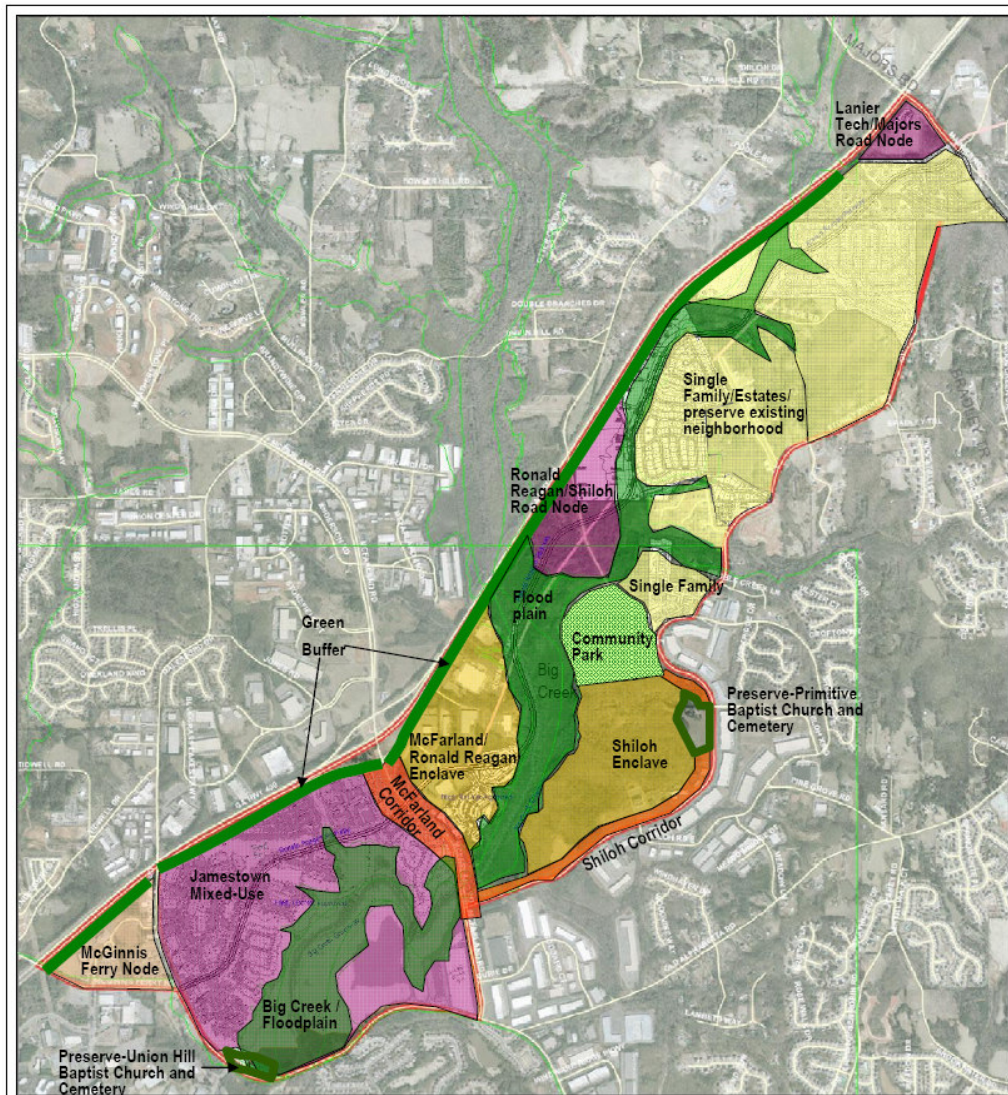
- McFarland Corridor
- Jamestown Mixed-Use Development
- Shiloh Corridor
- Shiloh Enclave
- McFarland/Ronald Reagan Enclave
- Single-family/Estates
- Ronald Reagan/Shiloh Road Node
- Community Park
- Ronald Reagan Boulevard
- Greenways/Multi-purpose/Bike Trails
- Lanier Tech/Majors Road Node
- McGinnis Ferry Node

The map on the following page presents the development area locations. Then, each area is further described with a detailed narrative, accompanied by detailed concept plans and typical sections as appropriate in order to communicate the components of the overall concept plan.



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Development Plan Map 1 of 2
Development Areas

McFarland – Union Hill LCI
 Forsyth County, Georgia
 Scale: 0 2500'
POND
 Company



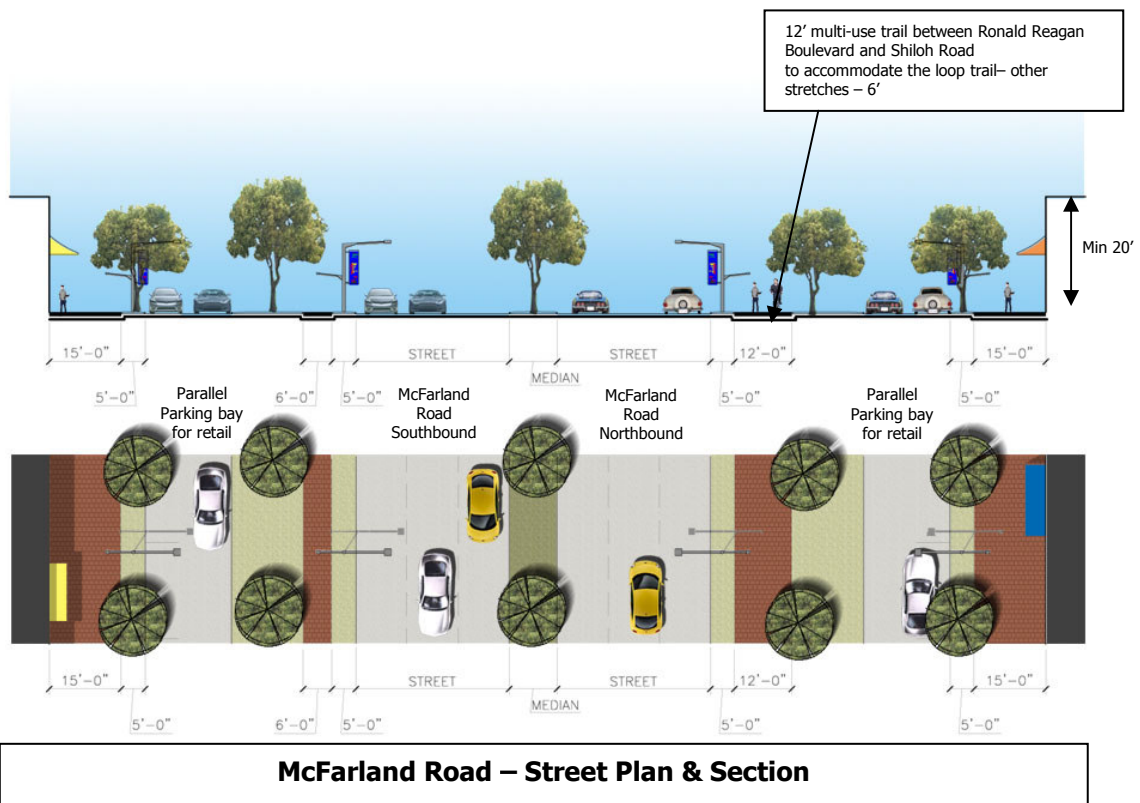
McFarland - Stoney Point Livable Center Initiative

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McFarland Corridor

McFarland Road being the major arterial and access to GA 400 provides the main access to the Study Area. This draws significant automobile traffic on the corridor. This wide multi-lane arterial road is conducive to retail uses along the corridor. This corridor is envisioned to support one- or two-story retail along both sides of the street with a well articulated landscape strip. Most of the parking is to be located at the back with a single bay of parking in front of the building facing McFarland Road that offers customers access to pull up in front of the building to make the retail successful. The additional right-of-way required for the parallel parking bay should be dedicated as right-of-way as new development is approved.



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Jamestown Mixed-use Development

The large tract bound by McFarland Road to the north, McGinnis Ferry Road to the south and GA 400 to the west is targeted for a mixed-use community with a variety of uses, retail, offices, variety of housing types and hotel/conference. The development should integrate Big Creek and provide public access to creek and greenway connectivity to the Big Creek Trail at various locations. It is imperative to create a very pedestrian friendly environment with smaller street grids, well articulated sidewalks, streetscape, open spaces, parks and public spaces. A series of hierarchical nodes should be created with a mixed-use village as the focal point for the entire community. The design of buildings and attention to the relationship between the buildings and streets are critical to create the spaces and visual image and aesthetic quality of the environment. The Union Hill Baptist Church and the cemetery are to be preserved. A suitable location should be identified for a potential future MARTA station at this center.

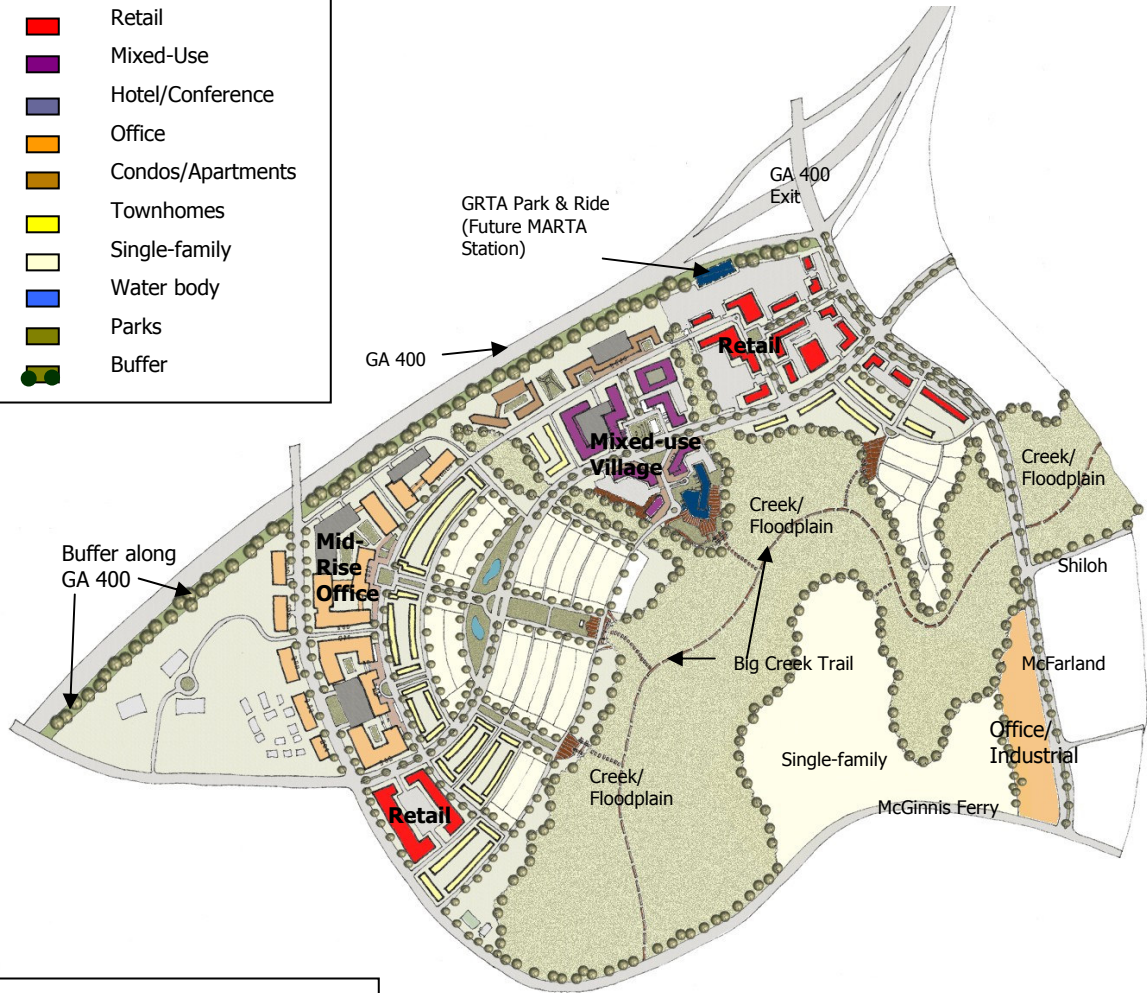


McFarland - Stoney Point Livable Center Initiative

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	Retail
	Mixed-Use
	Hotel/Conference
	Office
	Condos/Apartments
	Townhomes
	Single-family
	Water body
	Parks
	Buffer

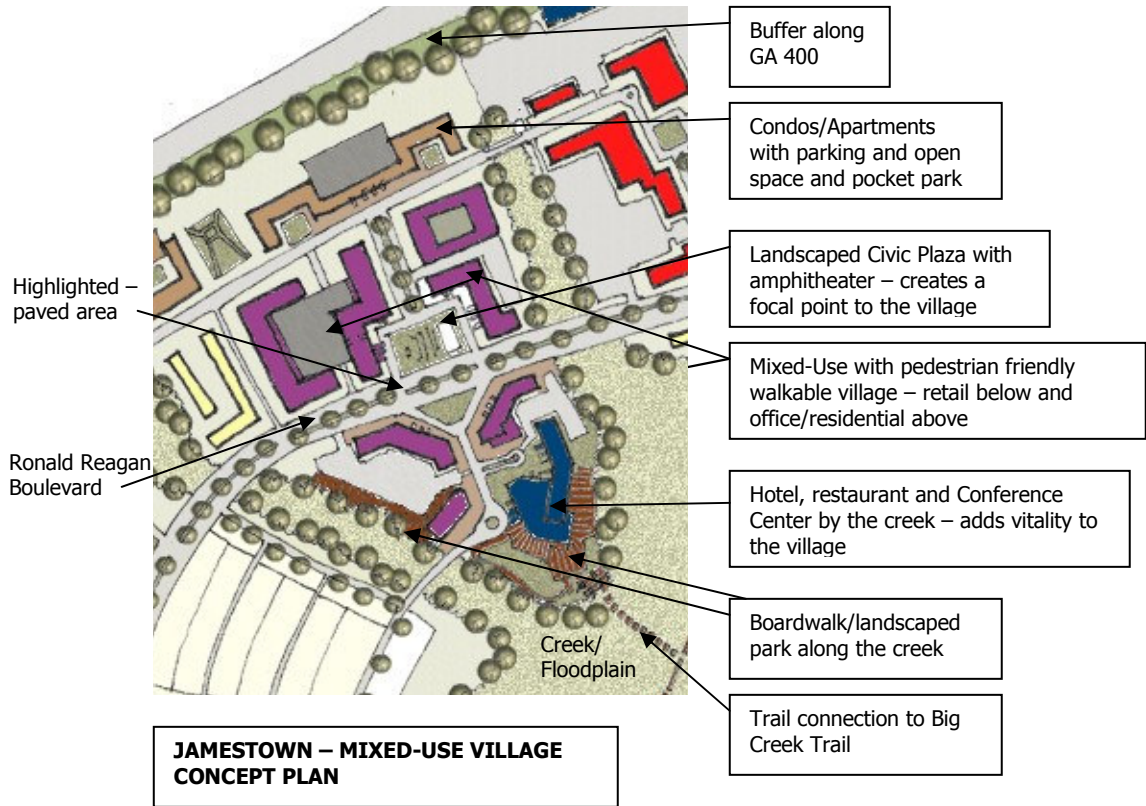


JAMESTOWN – MIXED-USE CONCEPT PLAN



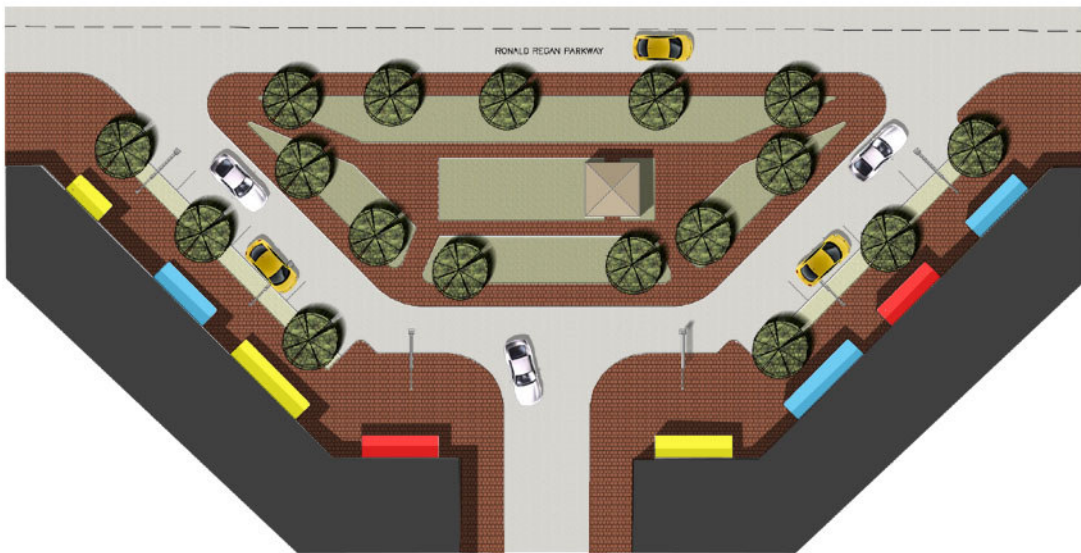
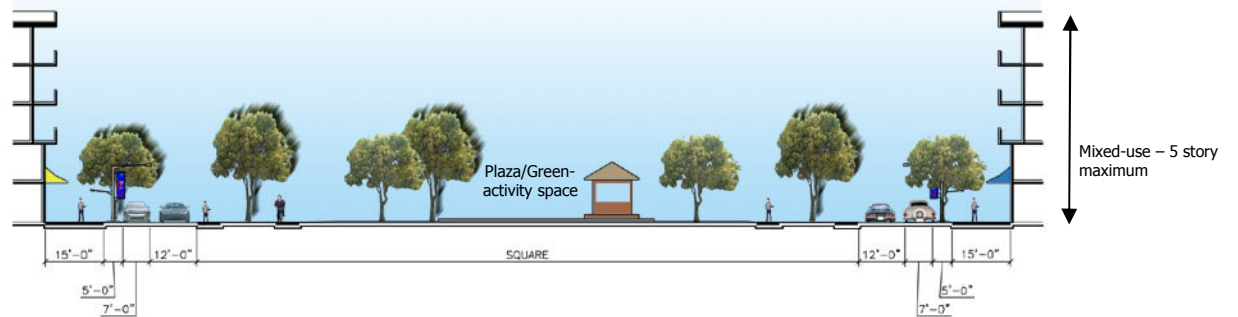
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MIXED-USE NODE @ VILLAGE – CONCEPT PLAN STREET SECTION



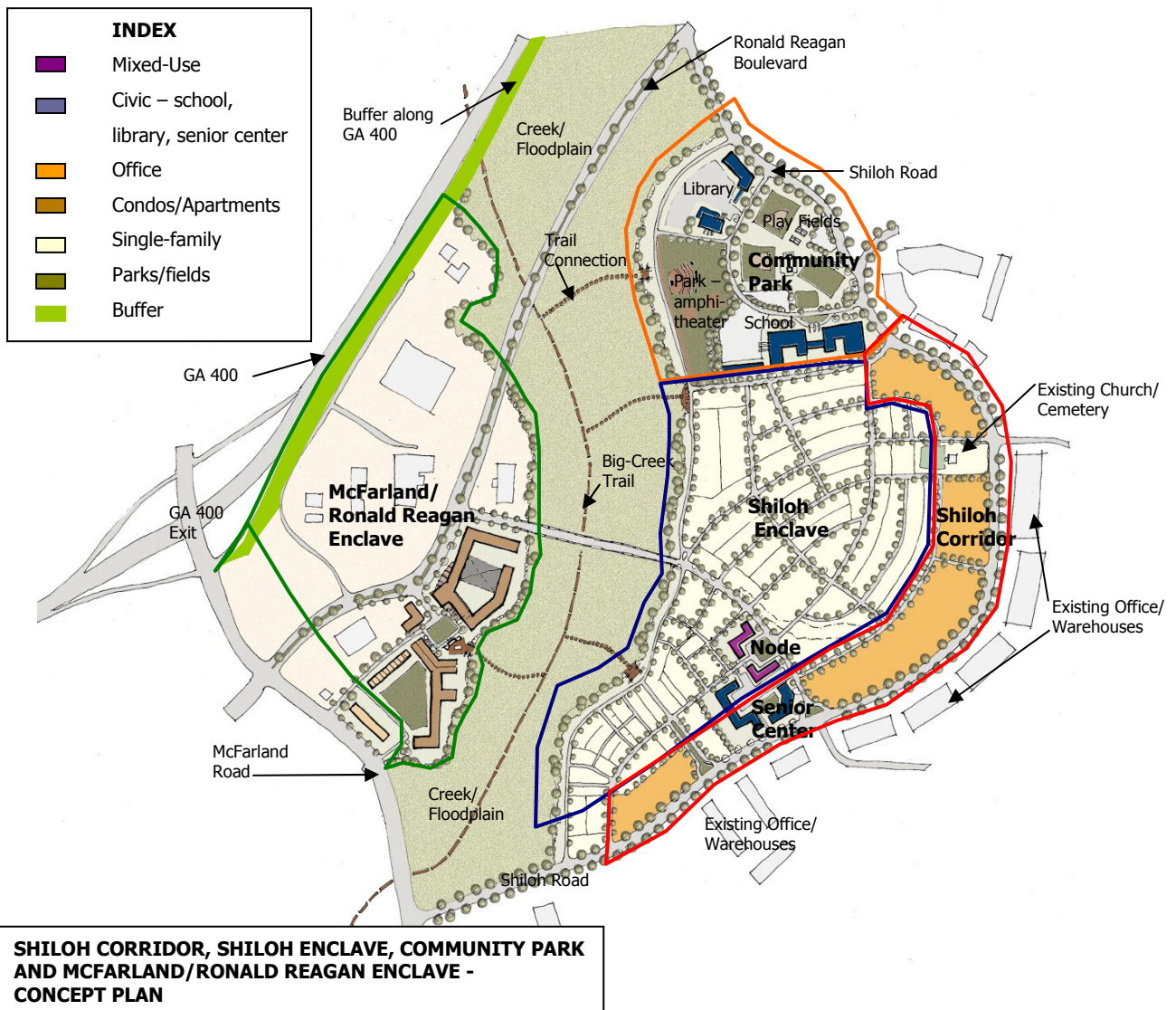
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Shiloh Corridor

The east side of Shiloh Road is presently lined with predominantly office/warehouse development. To relate to this use across the street, the west side of Shiloh Road that lies within the Study Area is proposed for mixed-use primarily office/commercial/condos that can offer flexibility in land use and also provide a transition to the residential behind. The Primitive Baptist Church and the cemetery are to be preserved.

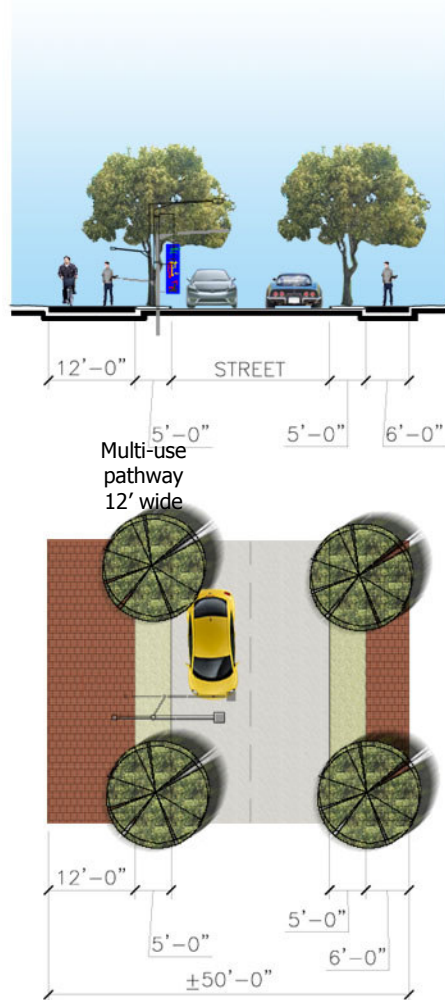


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The following typical cross section is recommended for both Shiloh Road and Stoney Point Road. This section may be constructed within 60 feet (minimum) of right-of-way. The community wants these streets to be designed as low-speed collector streets; thus, the roads should not be widened to support additional through lanes. Final design of this section must address numerous potential conflicts regarding topography (such as the existing trees along Shiloh Road) and traffic operations (including the provision of left-turn lanes). The key components of the section are the provision of adequate pedestrian buffers (landscaped, 5-ft minimum planting strips) and sidewalk/trail widths (6-ft and 12-ft, respectively). Ideally, the trail would be buffered at least 10-ft from the roadway, which may also serve the goal of preserving the existing trees along Shiloh Road; additional right-of-way will likely be required but this cost may well be justified. Similarly, the 6-ft sidewalk may be aligned to meander slightly in order to avoid the trees where necessary.



SHILOH AND STONEY POINT ROAD - STREET PLAN & SECTION

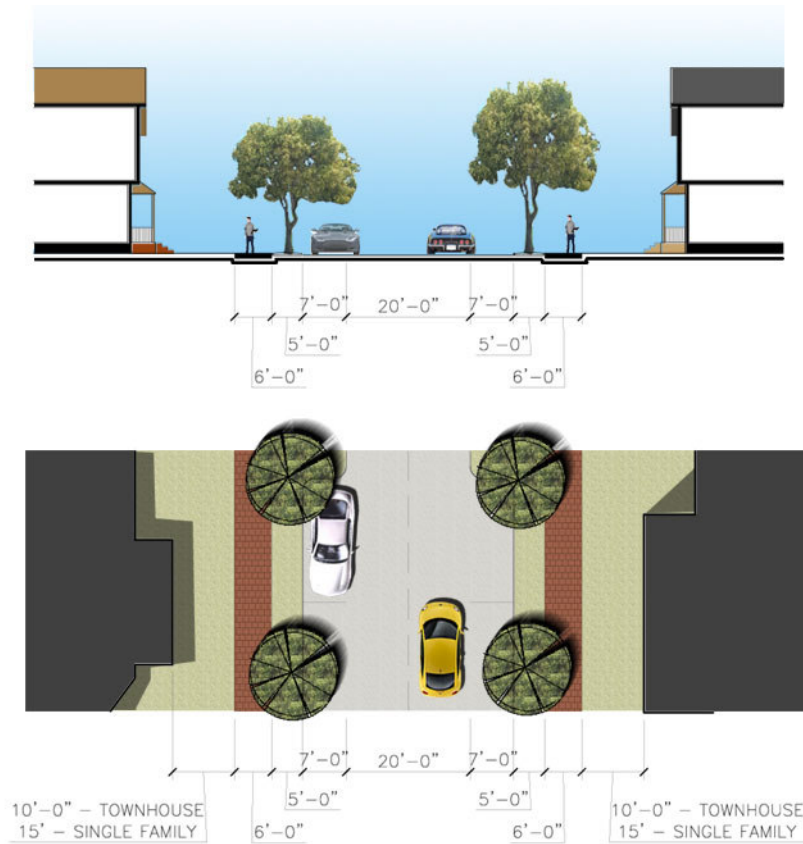


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The following typical section applies to new residential streets which private developers would be required to build to serve new subdivisions. This section represents a significant departure from present County engineering standards. Key components of this section include a reduction in travel lane width to 10-ft, 7-ft for parallel parking bays and bulb-outs, 5-ft planting strips, and 6-ft sidewalks. In order to realize the more livable community vision for the Study Area, these components should be incorporated into new County standards.



TYPICAL RESIDENTIAL STREET - STREET PLAN & SECTION



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Shiloh Enclave

This large tract abuts Big Creek on the western edge. This area is characterized by steep grades that slope down toward Big Creek. This area is proposed for medium density residential development of Single-family and townhomes with compact street network with pedestrian friendly streets with small pocket parks in close proximity to the residents.

McFarland/Ronald Reagan Enclave

This area lies between GA 400, Big Creek and McFarland Road. Presently there are many industrial uses in this area with very little vacant land available. The proposed future uses for this area are high density residential, office and commercial that benefits from good access to GA 400 in close proximity. High density residential should include apartments and condos that are located along Big Creek away from GA 400 and the commercial/office located between GA 400 and high density residential.

Single-family/Estates

The northern end of the Study Area is characterized by larger Single-family and estate homes. These residential areas are to be preserved and other vacant land around is proposed for lower density residential uses.

Ronald Reagan/Shiloh Road Node

This small node at the intersection of Ronald Reagan Boulevard and Stoney Point Road is proposed for mixed-use node with retail, office and high density residential; town homes and condos. This lies adjacent to the GA 400 that becomes a buffer or a transition from GA 400 to the residential to the east.

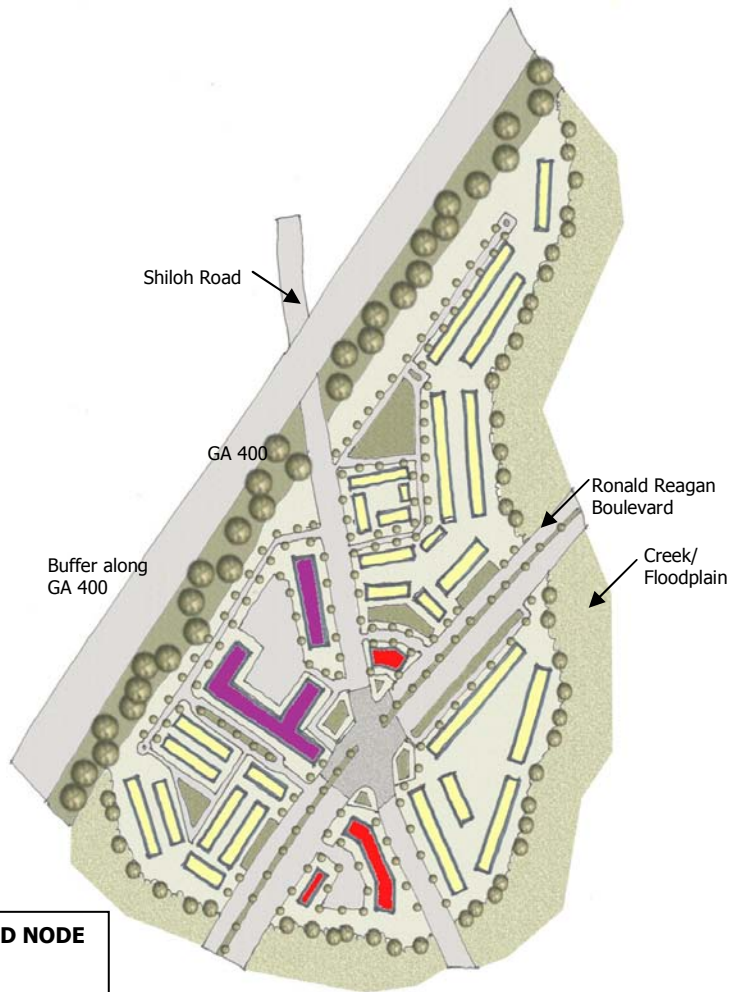


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Mixed-Use – Condos over retail
Retail
Townhomes
Buffer



**RONALD REAGAN-SHILOH ROAD NODE
- CONCEPT PLAN**



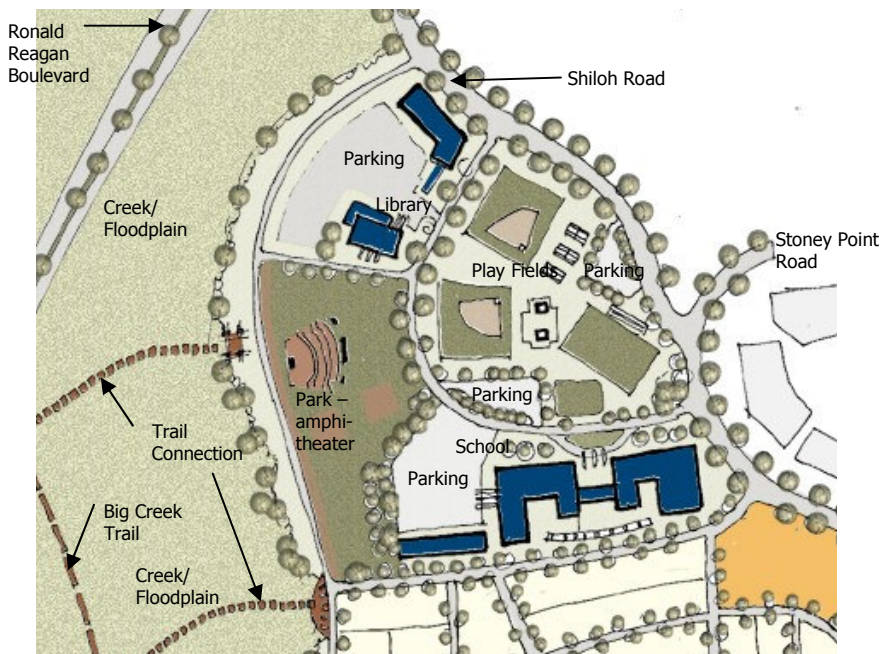
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Community Park

The area as shown in the concept plan is a suitable location for a community park that is in a central location of the residential community that surrounds it. It also abuts Big Creek which allows for integrating Big Creek with the park. At this location the grade drop is less dramatic to Big Creek than other locations within the Study Area. This park serves both active and passive recreational uses; both parks and various fields, such as baseball, football, basket ball, tennis, soccer, play lots, and potential civic uses such as library and also site for school. An amphitheatre is also proposed that can create a focal point to the community. Co-locating these uses can also better utilize the parking by sharing as well. About 20 acres of vacant land currently exists which could be acquired for the park. The other land that is currently used by industrial uses can be added as they phase out or the land gets redeveloped. The park and the facilities can be expanded as the land becomes available.



COMMUNITY PARK - CONCEPT PLAN



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Ronald Reagan Boulevard Corridor

Ronald Reagan Boulevard is envisioned as having two lanes in each direction separated by a landscaped median, a 12-ft multi-use trail on one side, and an 8-ft sidewalk on the other. A complete loop trail is envisioned which follows northbound Ronald Reagan Boulevard, Stoney Point Ridge, southbound Stoney Point Road and Shiloh Road, and westbound McFarland Road.

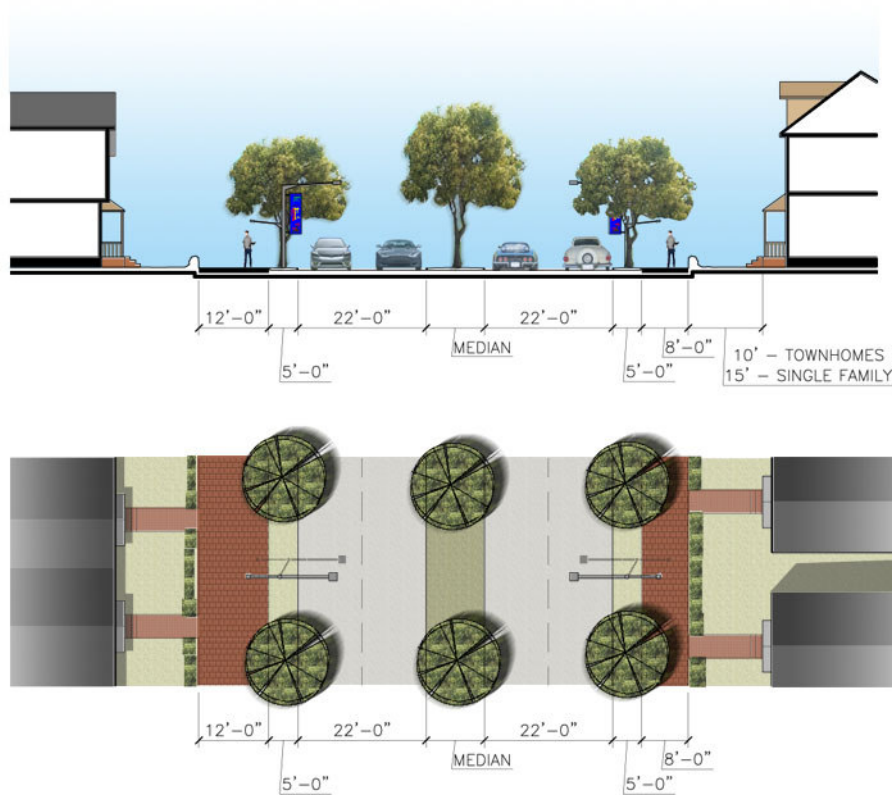
The recommended typical sections which follow represent a significant departure from the GDOT minimum standards which have been stated as the boulevard conceptual section. In order to attain the vision articulated by the community, a new concept is required. It is recommended that the County prepare and adopt a formal, conceptual plan for the entire length of Ronald Reagan Boulevard through the Study Area that is consistent with the vision and recommended design compendts communicated by this document. Because the boulevard is such a critical transportation element of the Study Area, more discussion and detail about the proposed alignment and typical section are provided in the Transportation Recommendations section.

As Ronald Reagan Boulevard traverses the Study Area from south to north, it will embrace different types of development areas, including: mixed-use node, high density residential, floodplain, and single-family/estates. The various characters of the boulevard are represented in the drawings that follow.



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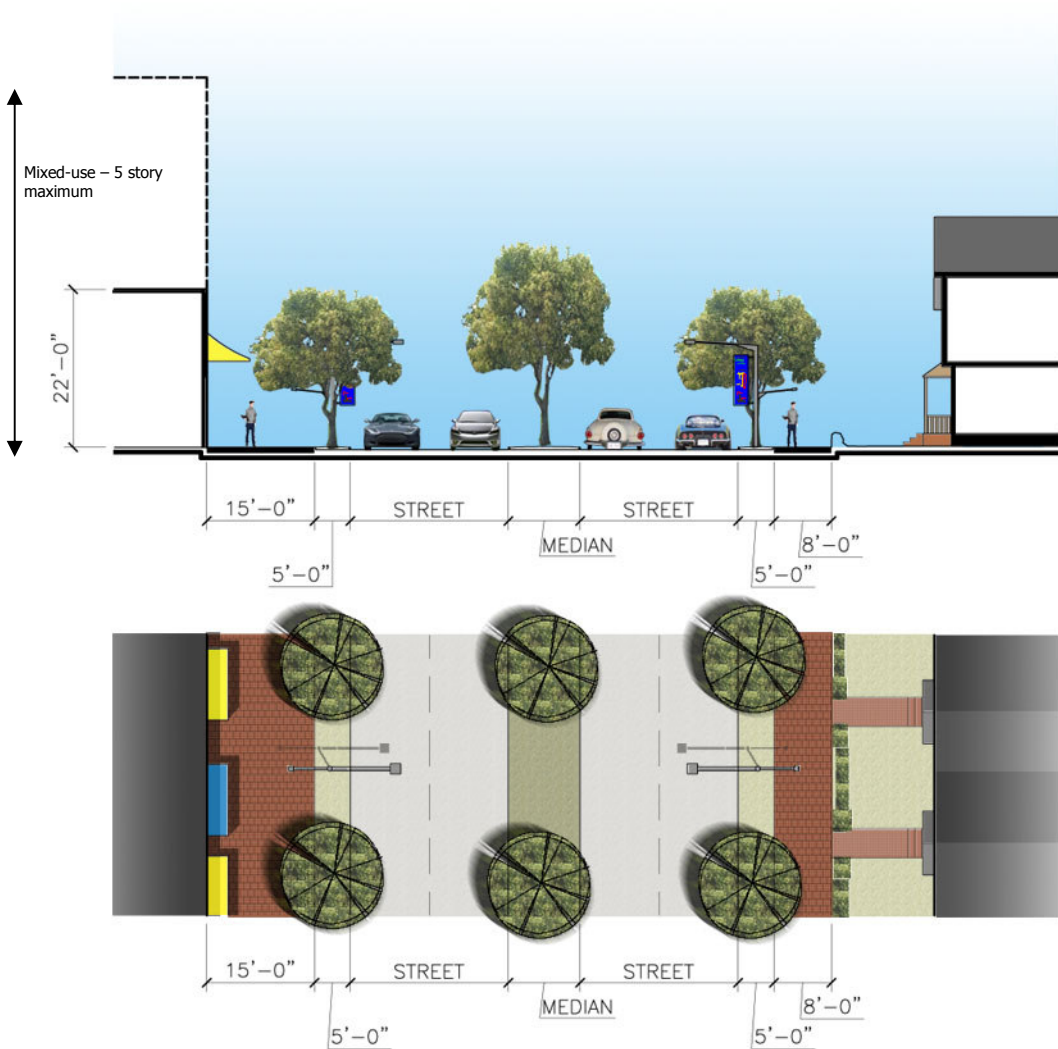
FORSYTH FORWARD



RONALD REAGAN BOULEVARD - STREET PLAN & SECTION – RESIDENTIAL ON BOTH SIDES



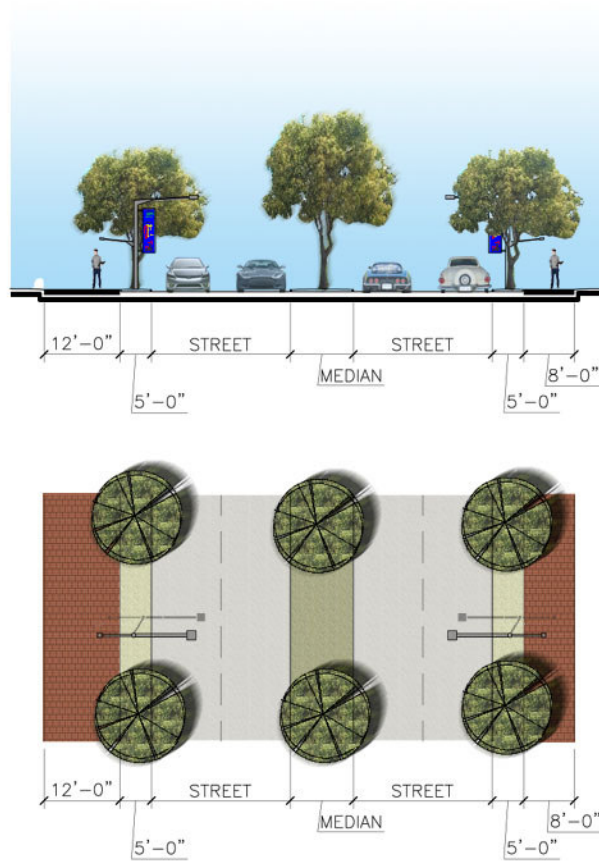
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**RONALD REAGAN BOULEVARD - STREET PLAN & SECTION –
 RESIDENTIAL ON ONE SIDE AND RETAIL/MIXED-USE ON THE OTHER SIDE**



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**RONALD REAGAN BOULEVARD - STREET PLAN & SECTION –
 FLOOD PLAIN/CREEK ON BOTH SIDES**



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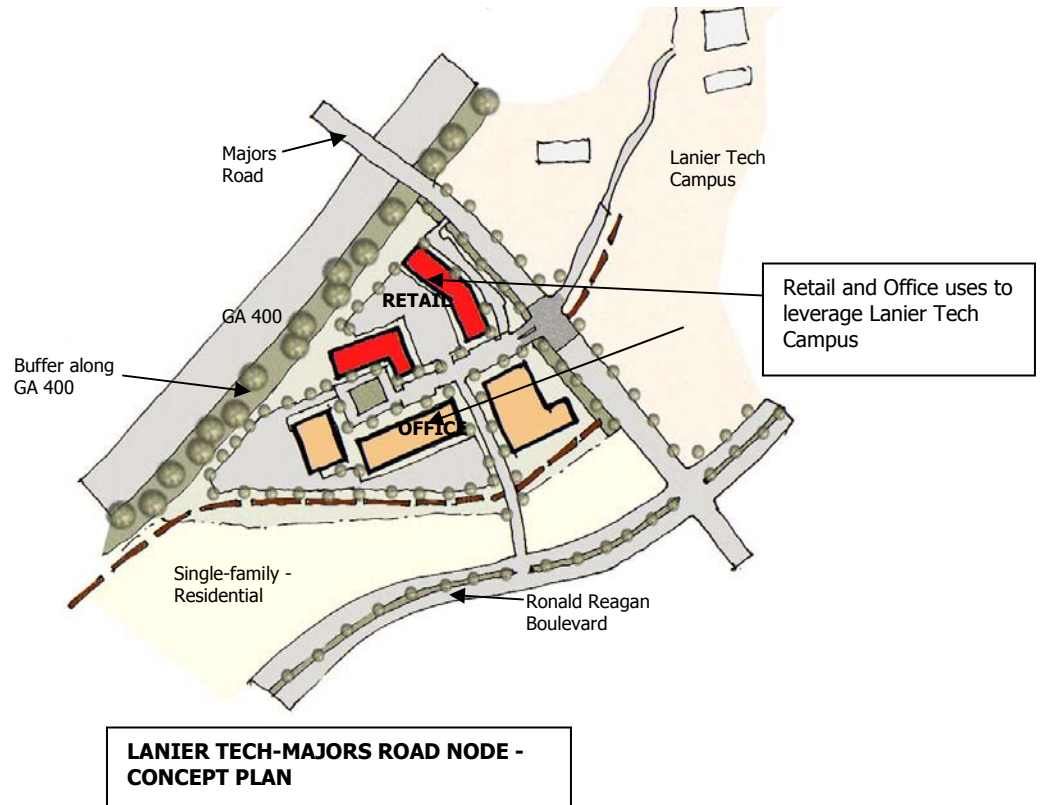


Greenways/Trails

A greenway system along Big Creek is proposed as shown in the drawing. A spur from the Big Creek Trail is proposed to be extending up to Lanier Tech Campus that provides connectivity throughout. All of the developments whether residential, office, mixed-use or retail that abut the creek should provide some connectivity to the Big Creek Trail and Greenway. Dedicated spots with entrances to these trails/greenways with parking should be provided.

Lanier Tech/Majors Road Node

At the northern end along Majors Road, a small node of retail/office is proposed that leverages the Lanier Tech Campus across the street. Retail services that cater to students and staff are proposed. Potential offices or business that can leverage the programs offered by Lanier Tech are also proposed at this node. The availability of land at this node limits the extent of services that can be provided.



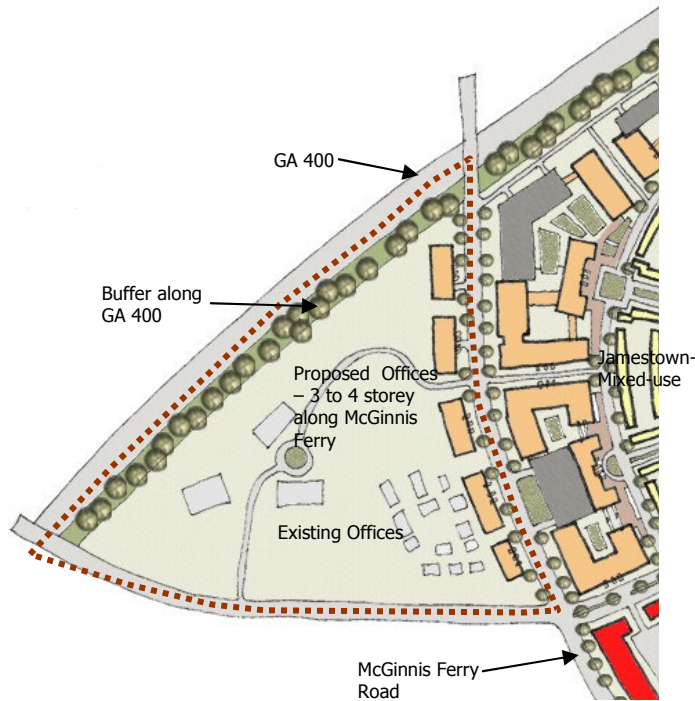
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McGinnis Ferry Node

This node currently contains office uses that are to be retained. The edge along the McGinnis Ferry Road across from the Jamestown mixed-use Development should be 3-4 stories that can relate to the mid-rise office buildings across the street.



MCGINNIS FERRY NODE - CONCEPT PLAN



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Gateways

Gateway treatments are proposed at the entrances to the community, at GA 400 and McFarland Road Interchange, Majors Road and Ronald Reagan Boulevard, Shiloh Road and McFarland Road intersection, Union Hill Road and McGinnis Ferry Road intersection, and Ronald Reagan Boulevard and McFarland Road intersection. The design elements for these gateways should reflect the rustic and quaint character of the community, such as horse farm fencing, etc. Pavers, lighting, signage markers, landscape elements should be integrated to define these gateways.

Transit

A green buffer should be provided along GA 400 which could be utilized for MARTA – transit as it expands in the future. An Express Bus Park & Ride station should be located in the Jamestown mixed-use Development; in the long-term this site could accommodate a MARTA station.



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25 YEAR DEVELOPMENT PROGRAM					
SUB AREAS	Site Area	Existing Use	Proposed Uses	Estimated Development Year	Comments
McFarland Corridor	30 acres	Retail and undeveloped land	Retail - surface parking	2005-2010	Private development
Jamestown Mixed-Use Development	370 acres	Undeveloped land	Retail, commercial/office, mixed-use, condos, townhomes and single family - surface/deck parking	2010	Public and private
Shiloh Corridor	55 acres	Office and undeveloped land	Mixed-Use - Convenient Retail, Office, Commercial, Condos - surface parking	2010-2015	Private development
Shiloh Enclave	154 acres	Undeveloped land	Medium density residential - single family and town homes closer to Shiloh Street abutting the mixed-use	2010-2015	Private development
McFarland/Ronald Reagan Enclave	135 acres	Industrial, retail and office and undeveloped land	Mixed Uses-high density residential, office/commercial and industrial-surface/deck parking	2015-2010	Private development
Single Family/Estates	468 acres	Single family residential	Low density single family	N/A	Preserve existing, private development
Ronald Reagan/Shiloh Node	80 acres	Undeveloped land	Mixed-Use - Convenient Retail, Office, Commercial, Condos, Town Homes - deck/surface parking	2007-2010	Private development
Community Park	61 acres	Industrial and undeveloped	Community park, library and school	2007-2015	Public and private
Ronald Reagan Parkway		Undeveloped land	N/A		Public and private
Greenway/trails		N/A	N/A		Public and private
Lanier Tech/Majors Road Node	20 acres	Retail and undeveloped land	Mixed-Use-retail/commercial/office	2006-2012	Private development
McGinnis Ferry Node	60 acres	Office and undeveloped land	Office/Industrial	2015-2020	Private development





Section C. Synthesis and Action Plan

Synthesized Assessment

This section first summarizes the challenges, assets, and key issues facing the development plan for the Study Area including those identified by the community, county staff, and the consulting team. Then, taking into consideration the existing conditions analysis and community vision for the future, a synthesized assessment of the situation is presented.

Issue Identification

There is significant potential for development in the Study Area. However, as in every community, there are challenges that need to be addressed and assets that need to be recognized. A consistent circumstance in terms of planning, market analysis and economic development is that, many times, issues are just opportunities in hiding; meaning: what seems like a negative might easily be turned into a positive for the community with an adjustment in perspective and a leveraging of resources.

That is why it is important to face them, recognize them, come to understand them, and implement actions to change them in order to move *Forsyth Forward* in the long-term. These issues and opportunities are based on stakeholder interviews, market assessment, and feedback at public meetings.

Challenges

- Travel distance to convenience goods and services
- Very small range in housing prices
- Poor east/west access
- Office market seems stagnant
- Little high-end retail amenities in area
- Perceived political environment
- Public sentiment and lack of experience with quality high density and mixed-use development
- Few for-lease options in housing

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- Workforce/Labor shortage in immediate area due to housing prices, could impact business location/relocation decisions
- Strong retail competition south and north of Study Area
- Public sentiment and lack of experience with quality low- and moderate-priced housing development options
- Traffic congestion along selected corridors
- Underutilized footprints
- Seemingly little coordinated business development in place that concentrates on recruitment
- Lack of connectivity
- Development activity not consistent throughout area
- Access and infrastructure issues are limiting types of development

Assets

- Undeveloped land can be proactively planned for
- Greenspace; opportunity to capitalize on greenway/trail connections
- More residential development, with options for diversified product
- Responsive county staff
- Support for a well-designed master plan similar to Johns Creek and Windward Parkway
- Residential base is well-educated, moderate- to high-income, potential decision-makers to make commercial location decisions
- Rapid residential growth in recent years, with trend expected to continue
- Leverage location and economic impact of Northside Hospital
- Proximity to regional recreational assets, such as North Georgia mountains and Lake Lanier
- Retention of rural community feel
- Proximity to higher education and career training opportunities and services
- Desirable residential area
- Programmed transportation improvements

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Issues

The following table condenses the challenges and assets into problem statements and issues categorized by traditional planning elements.

ISSUE IDENTIFICATION	Land Use / Development	Traffic & Transportation	Economic Development	Housing	Urban Design	Community Facilities
PROBLEM STATEMENT	Identify an appropriate mix of uses with reasonable densities	Improve transportation network by providing more sidewalks & bicycle paths; slow traffic	Provide adequate infrastructure to accommodate rapid growth; create campaign and incentives to attract quality development	Create housing choice; senior & affordable needed	Develop a community character & sense of place	Provide public gathering places & assembly opportunities
KEY ISSUES	1. What does dense development look like?	Can we slow cut-through traffic long Stoney Point Road?	Does the market support community vision?	1. Can we get housing affordable for college graduates & seniors?	1. What are appropriate design guidelines?	Can we get more parks & trails?
	2. How can the area achieve desired retail mix?	How will LCI & Ronald Reagan Parkway impact Stoney Point & other existing roads?	What are the area's business targets?	2. Can we preserve existing neighborhoods?	2. What should area look like?	Can we get libraries & pools?
	restaurants, coffee shops, family dining places; open air mall, accessory stores	Can area roads get shoulders & sidewalks?	How can we leverage Lake Lanier / Tourism?	3. What is a good mix of housing?	3. Can we maintain rural/ranch character	
	3. How can the area preserve greenspace?		How can we develop / diversify retail?			



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Assessing the Situation

Without a doubt, the Study Area is in a positive and dynamic position. With timely and appropriate infrastructure and access improvements, an opportunity is rising here. There are really two directions the Study Area can go in: (1) a more traditional route that supports single-family residential taking over the area, with low levels of convenience retail developing later; or (2) a more balanced direction that creates a mixed-use village destination area. At this point in time, the market could support either of these directions. It should be made very clear that a third direction of no growth or status quo is not a realistic option here, the growth is coming; the decision to be made is simply how it is going to happen.

This is a very enviable place for the community to be in, it does not often happen within the development process to have an opportunity to strategically consider the direction of future development as momentum is building. Many communities try to consider these elements before the market is really there to support it, or often times it is well after development has taken a path, and the community realizes it does not want to go in that direction after all.

These two paths are basically equal choices now; choosing to take one direction or another. However, these are not equal choices in terms of long-term development. The more traditional route mentioned usually means that when people want other choices, they move to another place. A more balanced direction provides choices to allow people to have other options and still remain part of the community they are in. What is seen as balanced now is, in truth, the more economically sustainable and viable option in the long-run. It is the one that offers a quality of life to residents and businesses alike, and will sustain its ability to be a destination for many years to come.



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Land Use Planning Considerations

The following is a discussion of planning considerations by land use (namely, residential, retail, and office/industrial).

PLANNING FOR RESIDENTIAL DEVELOPMENT

- Residential Market Demand Summary:
 - 1,250 units in 5 years;
 - 2,500 units in 10 years
 - Housing Diversity (single- & multi- family, detached, townhomes, condominiums, apartments)

Critical assumptions about tenure characteristics and housing preferences were made to produce potential demand. An increase in both multi-family units and single-family attached units are crucial to ensure quality development in the Study Area in the future. More densification of the residential base in the Study Area will help to attract more amenities and services to the area. Not only are there very few goods and services available within the Study Area now, this was cited as a concern in both public forums and Core Team/Committee meetings for this project. Additional residential product will then help to stabilize and improve market conditions for further office development.

The reality of the Study Area is that there is not an extensive market pressure to move towards mixed-use development and higher densities. This is really a key time for County leadership to make important decisions about the future of this area. As was already mentioned, we believe some direction can be interpreted from the approved developments in the area that include both multi-family and townhome components.

Until this year, the last multi-family permitted in the County was in 2000. The impact of this is starting to show across the County. In interviews, concerns about workforce size and availability for service and retail jobs was cited frequently. Part of the reason for the lack of commercial goods and services in the Study Area is not based solely on the size of the market, but also based on the availability of workers in the area. Obviously a business has concerns about opening or expanding if they are not sure they will have the workforce they need. This also impacts office development, and can help explain why that has been a slow developer in the Study Area. For-lease product is many times

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considered in a location decision for employers because it makes it easier to attract employees, particularly those relocating from other areas.

Further, without the addition of multi-family development, this area would certainly easily have a market to fill based solely on single-family home development. But, the long-term consequences of that, in a County that is predominated by single-family homes at present, is creating yet another bedroom community, with few amenities, longer drive times for residents, and a smaller tax base to fund County programs, improvements and initiatives from. Additionally, developing only single-family residential is in direct contrast to the stated Land Use Community Goals in the Forsyth County Comprehensive Plan, as well as the land use policies set forth in the McFarland Subarea Land Use Report.

Single-family residential is still a needed and viable component of our recommended future development, but it should be as one part of an overall housing program. Housing product diversification is key for the future of this area, in order to attract commercial uses that will help fund its existence. Further, in the LCI application, it was clearly stated that the impetus for the application was for this area to be a model for the rest of Forsyth County and the way it could develop. Given its exurban location, and large proportion of undeveloped land compared to the rest of the metro area, it is a logical and sustainable approach to the residential market to create more choices for residents.

Certainly, some diversity in residential development is already proving itself within the County, in the example of Vickery. It has had stellar sales reports, and they can not build fast enough to keep up with demand of customers. Their commercial uses are filling up, and the types of goods and services and amenities available within a walk-able village has been a critical selling point for the development. Our recommendations revolve around the simple concept of ensuring more choices to appeal to a greater diversity of customers. There is a clear consumer preference shift happening in the residential market, as more and more people want to buy a lifestyle in a neighborhood, not simply a house in a subdivision. Again, by increasing that customer base, it increases the commercial development that will want to make its way into that area based on the target market characteristics. As new projects start, more interest and momentum is developed, thus, it is highly likely that more demand will also be developed for residential product within the Study Area.

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PLANNING FOR RETAIL DEVELOPMENT

- Retail Market Demand Summary:
 - 565,655 sq ft at present;
 - 736,490 sq ft in 5 years
 - Convenience, Regional/Chain, Regional/Unique

Because there is such a dearth of retail product in the Study Area, there is opportunity for all three types of retail mentioned in the retail market analysis section: Convenience; Regional/Chain; and Regional/Unique. Important elements in the retail market are growth rates and income statistics, both of which are above the national average and the Atlanta MSA for the Study Area, as detailed in the Socioeconomic Analysis section. Convenience retail will most likely naturally develop as the residential base increases. The proposed Ronald Reagan Boulevard also is a factor here, as it could help demarcate and significantly impact desirable locations for retail establishments. There is a smaller opportunity in the Study Area for Regional/Chain, based on competitive locations of Regional/Chain uses in the Windward Parkway/Alpharetta and Cumming areas. The reality is that there will always be retail leakage from the Study Area because of its proximity to North Point Mall, Windward Parkway, Cumming and Dawsonville.

Additionally, the development now under construction at Old Milton Parkway and GA 400, just south of the Study Area, is going to be significant competition. Prospect Park is a mixed-use project that will include: retail space, including movie theater and restaurant; commercial space, including office/hotel/civic uses; and residential space, including a six-story complex with 472 units and another complex with 230 units at 10 units per acre. The Alpharetta Planning Department required, as part of approval, that the retail tenants be “high end and comparable to those . . . currently located within Phipps Plaza and Lenox Mall.”

As previously mentioned, there is not an extensive market pressure to move towards mixed-use development in the Study Area. This is really a key time for County leadership to make important decisions about the future of this area. The area could run the traditional track of increased single-family residential development, which will eventually attract some Convenience retail and, depending on size, attract some Regional/Chain retail. But, that will be long-term development that would, again, create longer drive times for residents and a sprawled development lay-out that does not

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efficiently use land or increase quality of life. Additionally, it would be against the stated Livable Centers Initiative goals, as well as Forsyth County's own Comprehensive Plan.

Based on the approach to the Study Area as eventually serving as a model for Forsyth County and the Region as a whole, there is an opportunity for Regional/Unique retail. This is about creating destination retail that increases the choices that people have both inside the Study Area and outside. Mixed-use development that confirms the existing customer base and increases the Primary Market Area draw is what is needed. Again, the idea of a village concept with multiple purposes for destination can help to leverage retail tenant attraction. Having a central location that allows residents, employees and visitors at all different times of day and times of the week to have a purpose to be there can leverage other trips to adjacent uses. Because of the undeveloped land, the access to GA 400 and the improving transportation network, there is a distinct opportunity to create mixed-use development on a scale that has not been done in a developing exurban location in metro Atlanta to date.

PLANNING FOR OFFICE/INDUSTRIAL DEVELOPMENT

- Office/Industrial
 - 932,350 sq ft in 5 years ;
 - 2.17 million sq ft in 10 years

Continued business park development within the already identified areas makes sense within the Study Area. However, the new trend of office condos seems to be entering this market already. Within the Study Area, The Corporate Pointe Business Park at The Meadows is leveraging this new product. The developer will actually allow larger than average space allotments for the office condos, but expects tenants to not only be industrial clients, but involve commercial and service-based tenants. The level of amenities offered at this facility is above what has been done with developments within The Meadows previously.

This is an important example, because it helps point to the direction future office development within the Study Area will most likely take. The addition of office condos will keep up with trends in the greater market area, and the transition of product type. Additionally, there should be a movement away from industrial and business park development, though that will still remain a viable use. An

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addition to this market of higher-class office space, including multistoried and multi-tenanted buildings, is expected.

Another trend that is happening nationwide, and can also be seen in the Study Area, is office space within mixed-use developments. This type of space usually targets professional services that need small spaces, and do not necessarily need storefronts; these would be a good fit for the Study Area. Another consideration is the trend that is being witnessed across the country in the combination of live and work space. This could be a solid direction for the office space development in the Study Area to pursue in the long-term as the residential base increases and the development in the area matures.

As previously mentioned, the increase in the residential base will help attract more goods and services to the area. As that happens, there will be a greater level of amenities in the area as well, which certainly can be leveraged as a positive characteristic for continued office development. A higher level of amenities in the Study Area will also help to promote a diversified tenant base, and help to increase demand for different types and higher classes of office product.

STEERING THE LAND USE MARKET

Without proactively addressing land use policy, Forsyth County will not realize the economic development potential outlined in the market study in a timely fashion. True, the housing will come along (as has recently been proven) regardless of land use changes, but it will occur incrementally and will be located predominantly in the northern part of the Study Area. Ronald Reagan Boulevard would be incrementally constructed as well and may only connect to the new subdivisions with no real economic gain for the county. The single-family residential base population, once established, may actually fight construction of the roadway.

The County should encourage the market to move forward through land use changes in the southern end of the Study Area, especially at the Jamestown Mixed-Use Development (between Union Hill Road / McGinnis Ferry Road and McFarland Road). Adopting regulatory changes in the area would constitute economic stimulus. Increasing the allowed diversity and intensity of development would

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add value to the land and ultimately increase the willingness of developers to construct the boulevard and street grid envisioned by the community. The retail component of the desired activity center will locate where there is an adequate volume of traffic, which the boulevard project would facilitate and, in the future, the McGinnis Ferry Road / GA 400 Interchange would ensure. Besides the boulevard, the connections made possible by a complete local street network are critical to ensure that the desired quality of retail and office components come to fruition. A conference center and retail destination will not settle for a site lacking multiple points of access.

Just as GA 400 has funneled growth northward from Fulton County into Forsyth County, Ronald Reagan Boulevard will ultimately drive development pressure northward through the McFarland/ Reagan Enclave, the Reagan/Shiloh Road Node, and ultimately the Lanier Tech/Majors Road Node. However, this progression will not occur without changes to the corresponding land use policy.



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Recommendations toward a Livable Center

An implication of the LCI program is that recent regional development patterns (characterized as low density, single use, and auto-dependent) have contributed significantly to unlivable conditions (including air quality problems, unacceptable levels of congestion, and inequitable transportation options). Regional planners have determined that the concentration of development in compact centers and along corridors where adequate infrastructure (including transit) is or may be made available would significantly decrease the strain on the regional road network.

The Study Area does not have a transit station around which development may orient. Still, given the lack of alternate corridors, future transit will most likely make use of the existing rights of way of freeways and arterials. The GA 400 / McFarland Road Interchange is not only the primary gateway into the Study Area but also the prime site for future transit service.

The same corridors which may one day support transit also provide the daily high traffic and high visibility required by major retail, which in turn serves as an amenity critical to development of a mixed-use activity center. Thus, it is prudent to plan for pedestrian-oriented development of the greatest diversity and intensity (politically feasible) in close proximity to the GA 400 / McFarland Road Interchange.

Arterials such as McFarland Road, however, are by traditional design not pedestrian-friendly, as they must accommodate high volumes of high speed traffic. Thus, local access streets must be provided as viable alternatives to the arterials for local trips. It so happens that such parallel streets are also a key component of arterial access management as they relieve the arterials of local traffic. Thus, a tight, inter-connected network of pedestrian-friendly, local access streets is a primary recommendation of this study to address the LCI goals of internal mobility, inter-parcel connectivity, access management, and alternate transportation circulation.

The following sections of this document present detailed economic development, land use, and transportation recommendations. Ultimately, the recommendations were constructed not only to capture the land use market in the Study Area, but also to direct it to locations deemed suitable by the local community. Adoption of this plan and implementation of the recommended actions will lay the foundation for a vibrant and sustainable activity center to emerge in the Study Area.

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How the Plan Addresses Livable Centers Initiative Components

1. *Efficiency/feasibility of land uses:*
 - Promotes mix of residential, retail, office, and institutional development
 - Offers regulatory changes to support mixed-use and housing choice
2. *Transportation demand reduction measures:*
 - Focuses more intense development in nodes at community-identified locations
3. *Internal mobility requirements:*
 - Codifies pedestrian-oriented design principles
 - Promotes access management of arterials and traffic calming of residential collectors
4. *Mixed-income housing, job/housing match and social issues:*
 - Promotes housing diversity through inclusion of multi-family units
 - Preserves existing and recently approved single-family residential use
5. *Continuity of local streets:*
 - Calls for a complete local network of streets, sidewalks and trails
6. *Need for future transit circulation systems:*
 - Supports plans for express bus service and future regional transit
7. *Connectivity to other centers:*
 - Promotes connections to Lanier Tech and Northside Hospital
 - Enhances access to GA 400 corridor
8. *Organization, management, promotion, and economic restructuring:*
 - Promotes pro-active Forsyth County adoption, investment, and oversight to ensure incorporation of community goals
9. *Stakeholder participation and support:*
 - Interviews and workshops
 - Commitment to recruiting participation by planning team
10. *Public and private investment policy*
 - Promotes public commitment of resources
 - Supports private development design principles



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Recommendations: Economic Development

The following discussion highlights the recommendations of this study particularly focused on economic development. A discussion of catalyst projects is followed by a list of recommended actions.

Catalyst Projects

There are many projects and initiatives that can be undertaken in efforts to improve upon the assets of the Study Area and continue current development efforts. It is important to strategically use public resources to leverage private investments. There are some projects that, when begun, can send the message to private developers, future residents, brokers and realtors, future businesses and existing Forsyth County residents and workers that something is really happening in the Study Area. The problem continually cited with developing plans and studies is that they sit on the shelf.

With that said, the projects suggested below should be viewed as the key programs, initiatives and projects that need to be priorities for Forsyth County in relation to the Study Area. These projects have the ability to set the Study Area apart, define its character, help it to become a destination and continue positive economic trends. Some are new developments that will be long-term efforts and some are leveraging existing assets to their fullest potential. Regardless of the horizon or development timeline, action must be taken today to get these projects underway. Again, there are a multitude of projects and programs that can help to move the Study Area forward, the projects below were selected based on market conditions, stakeholder interviews, potential to spur continued development, and leveraging strategic public investments.

Overall Direction: Creating Choices

As mentioned throughout this document, the reality is that there is not an extensive market pressure to move towards mixed-use development and higher densities in the Study Area.

What is a reality is that this is really a key time for County leadership to make important decisions about the future of this area. It should be made very clear that a direction of no growth or status quo is not a realistic option here, the growth is coming; the decision to be made is simply how it is going

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to happen and how best to guide it. As was already mentioned, the consulting team is interpreting some direction from the approved developments in the area that include multi-family residential, townhomes, and mixed-use development.

A decision could be made to take the traditional route of development. The Study Area would certainly easily have a market to fill based solely on single-family home development. But, the long-term consequences of that, in a County that is predominated by single-family homes, is creating another bedroom community, with few amenities, longer drive times for residents, and a smaller tax base to fund County programs, improvements and initiatives from. Further, retail would ultimately develop, but very slowly, and at a low level. It would have to locate a drivable distance from the residential area. Workforce issues would surface that could limit any kind of commercial development, as explained in earlier sections. Office development would continue to be slow in the area. The bottom line of this approach is that there would be missed opportunities to create a balanced approach and diversified tax base.

Another possibility is a decision to take a more balanced approach to development in the Study Area. In the LCI application, it was clearly stated that the idea for this area is to be a model for the rest of Forsyth County and the way it could develop. Given its exurban location, and large proportion of undeveloped land, it is a logical and sustainable approach to the marketplace to create more choices for residents and businesses alike. This approach does not preclude single-family residential development or business park development. It simply increases the number and types of choices that people can have in the area. As that happens, interest in the area increases and momentum is built.

The consulting team suggests a directional change for this area. The Study Area is basically a “donut hole” between Windward Parkway and North Point to the south and Cumming and Dawsonville to the north. Because of early infrastructure limitations, development essentially “skipped over” this area and kept going northward. The role of the construction of Ronald Reagan Boulevard is imperative for quality growth in the Study Area. It should be done on an aggressive timeline. The timely construction of the Boulevard is an underlying assumption in all the estimates and projections for future growth and development recommendations in this report.

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Since the Study Area is largely undeveloped, and its uses are predominately industrial and office, a spark is needed to bring people to the area. While improvements are being made, both transportation- and development-oriented, there has to be a key catalyst to help re-focus people on the area and its potential. The key here is to fight the natural inertia to keep doing the same thing, because it seems to work in the short-term. Instead, it is crucial to start think through decisions based on long-term vision and desires to achieve sustainability over many years.

This decision on direction for the Study Area is a critical factor in, and a basis for, the subsequent selection of catalyst projects and initiatives.

Create a Mixed-Use Village Destination

A true economic destination is still lacking in Forsyth County. The only city in the County is Cumming, and its downtown is not a key destination for residents, visitors or employees. A mixed-use development with retail, housing and open space would be a substantial catalyst to ignite this area.

Depending on the final decision of uses and scale, there appears to be opportunity to position the land for this development which would be worth the investment and potential headache. As was cited in the Retail Market Analysis section, there is sufficient demand to support additional retail space in the Study Area. Since there is such a small supply currently, the types of retail needed is relatively open. But, both convenience and destination retail should be considered, for both neighborhood-serving and community-serving needs. Restaurants are a must in this area. Retail space on the scale of a total of 250,000 square feet would be appropriate; this would not need to be in a single shopping center, but rather split into a primary cluster, with a few small spaces integrated at a neighborhood scale as well. Unique destinations, such as a children's museum, theatres or galleries, would be additional amenities.

There is also a need for a housing product diversification in this area; this simply means allowing mixed products and a variety of price points. This diversity is what can make an area thrive. Single-family homes, townhomes, condos and apartments should all be allowed to develop here. They should be co-located, and not separated into clusters. This will enable life cycle housing, meaning

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allowing recent college grads with their first job to couples starting families to retirees to live in the same community, and in close proximity to each other.

Another important component of housing product diversification in this area would include capitalizing on the trend towards active adult communities. This is ideal for baby boomers that are aging that might desire to be near their children and grandchildren, and become part of a greater community. Considering assisted living options within these settings would also be advisable for the Study Area.

Additionally, a configuration that allows live/work space and potentially street level retail in a portion of the development would be ideal. Townhomes are also an accepted and known product that can help bridge the gap between single-family and multi-family. They are a variable product type because they will provide a comparable scale that can help to transition to the surrounding single-family developments coming online. Additionally, having housing within this village concept helps to provide more street life for longer hours, which helps to improve the attractiveness of the area to both residents and consumers, as well as developers and retailers.

Further, there is a need for greenspace and a gathering place in the Study Area. Certainly the work being done currently on the Big Creek Greenway Multi-Use Path will have a significant positive impact on the Study Area. Parks should be developed, both small and large scale. Small parks could be an asset for shoppers or diners that take a stroll through the village after their meal or shopping trip. The possibility of an amphitheatre could help to generate trips that could benefit retail locations in the area.

The village concept is particularly important for long-term sustainability and viability. It helps to provide a reason to stay and re-invest in the community in this time of transience and mobility; opening options to people of every walk of life. Vinings in Cobb County is a good example of this. The core village has been there for many years, and was initially surrounded by only residential. Slowly office began to develop, and commercial uses followed. Over time, it has become a dynamic and vibrant place where people truly have options to live, work and play. It is so popular and well-known that areas as far away as 30 miles include "Vinings" in their subdivision, shopping center, and

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development names. The village itself becomes the amenity and identity that holds value for the community, both financially and emotionally.

Conference Center

A conference center would be a significant asset to the Study Area, particularly in terms of helping to create a destination location. To be clear, the recommendation here is not a large-scale civic center, but a small-scale conference center that would host local and regional business meetings, leadership retreats and social gatherings.

It would be appropriate for a hotel or lodge component to be included in this development. The need for hotels was cited by both developers and business owners who were interviewed, as well as Northside Hospital. When they have clients or out of town employees come in, they must stay at Windward Parkway or Johns Creek, there is nothing immediately close-by as an option.

This conference center location could help anchor the mixed-use village concept cited above. Additionally, it could leverage the Big Creek Watershed Area, as well as the upcoming greenway. Being close to a walkable village, with restaurants and retail amenities, would be a selling point for the conference center as well.

Develop and Implement Business Recruitment Strategy

There has not been a clear or discernable framework for business development in the Study Area or in Forsyth County as a whole. That is not to say that the efforts of Forsyth County, City of Cumming, or the Cumming/Forsyth County Chamber of Commerce have not been beneficial. But, this has left the overall vision and direction of economic development and the business mix to happenstance. The quality of the businesses in the Study Area is good.

In order for the local business climate and economic development to thrive and expand, there has to be a plan. A plan should include: the kind of businesses desired; specific tenants that would meet those parameters; quality marketing materials oriented towards attracting new businesses; a recruitment team that deals with inquiries as well as pursuing targets; and benchmarks to guide the

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process. There has to be clear role and responsibility assignments to ensure quality work is being done, and that there are no gaps or overlaps in service delivery.

Further, there also has to be a system in place to serve existing businesses and ensure they are successful, that their needs are being met, and that as the diversification of business mix happens, all businesses are able to achieve a fair market share and friendly competition can be handled within the marketplace.

Leverage Lanier Tech

The Forsyth campus of Lanier Technical College is located near the northern end of the Study Area, on Majors Road. Its campus is sizable and it is certainly a destination, bringing students from North Fulton, Forsyth, Dawson and Lumpkin counties. The diversity of class offerings ranges from technical education in a variety of occupational disciplines to lifelong learning educational opportunities. The four primary areas of concentration: Allied Health; Business and Computer; Public Service; and Technical and Industry, are competitive elements in today's economy.

Additionally, many people do not realize that the Georgia Department of Technical and Adult Education sites Economic Development offices at each of its member institutions. Thus, Lanier Technical College also offers services to local business and industry. Continuing education, safety classes, and customized training are some of the services they can offer to both existing and new industry. Having a conference center with lodging options would also be a valuable tool in trying to recruit new business or providing specialized training classes.

To leverage this valuable asset, it would be advisable to conduct visits to existing industry to assess needs, as well as better advertise the types of services Lanier Technical College provides that can become a competitive advantage for the area. This would be applicable from both a business retention standpoint, as well as a great recruitment tool. Additionally, the campus itself could become more of an asset to the area, with better connections and integrations as development happens close to the campus. Because an educational campus is a destination unto itself, trying to capitalize on the students that visit this campus on a daily basis, and at various times of day, should be done. They can become consumers for the retail uses in the mixed-use development village and could also become renters, and potential owners, within the variety of housing options.

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Leverage Hospital

No one would disagree that Northside Hospital-Forsyth is a great asset in the community. But, many people view it simply as a customer, meaning it is viewed as being “good to have a hospital so close so that I or a loved one will be taken care of nearby.” But, a hospital has a huge economic impact on its surrounding community. When Northside took over the hospital in 2002, they purchased it at the highest price per bed in the nation to date. They purchased it because they wanted to be closer to their patients in the Northern Crescent, its easy access to GA 400, and the anticipated growth in the area.

Northside Hospital-Forsyth, as many other businesses in the area, has been surprised by how rapidly the growth has occurred. They are actively expanding their facilities to meet demand, with approximately \$100 million in improvements planned. That has an immediate economic impact. The hospital does not only serve needs of its patients, it serves the greater community as a major employer, major buyer, and major player. They employ people for service jobs, for medical jobs, and for construction jobs. Medical facilities generate jobs outside of the medical profession, meaning maintenance workers, administrative workers, construction workers, and food workers, just to name a few.

Much of this economic impact is currently missing now in Forsyth County. Healthcare is frequently overlooked as an economic engine, but it is a viable one. Many of the hospital’s workers do not live in Forsyth; since it is a 24/7 operation, the traffic and congestion that deters many does not affect these workers. Because these are quality jobs, people are willing to drive from elsewhere to obtain them. Efforts should be made to make the proportion of local employees higher, whether through targeted workforce training in conjunction with Lanier Tech or further outreach efforts. The hospital could be a significant indirect client of a hotel (mentioned earlier in relation to the conference center), as many people would like to stay nearby while a loved one is in the hospital or for visiting medical personnel. There is also the possibility for meetings or training classes to be held at a nearby conference center facility.

There are two significant opportunities to leverage in conjunction with Northside Hospital-Forsyth. One is the ancillary, or spin-off, industries that occur with the healthcare industry. Currently, this is

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primarily physician-based, as doctors open offices nearby, but there is greater opportunity for further development to happen. Ancillary industries that could be a fit for the Study Area include: medical devices, pharmaceuticals, patient monitoring, home healthcare services, information systems, medical imaging, and laboratories. Part of the delay is the rate of growth in the area, more will naturally occur over time, but there is opportunity to actively recruit and attract these types of businesses to the area to continue building upon the asset. Northside does have another facility in Alpharetta, but it is a medical campus, not a hospital. This location in Forsyth will be a resource rich location that will have to continue to expand to serve the growing population in the greater community.

The other great opportunity with Northside Hospital-Forsyth is to have an assisted living development located near the hospital within the Study Area. The trend seen in these types of developments today is not the high-rises as seen previously, but rather villages. The housing is one-story, and people live in smaller homes in these developments that have group amenities. They are able to live within the community they are from, or be active participants if it is a new community for them. They still have access to goods and services in the community, and high levels of amenities within their own developments. Facilities and personnel are present to assist when needed. There are phasing options and transitions available within the same community for people to get the medical care they need at the time. Having a major hospital facility near this type of community is a significant asset and certainly a point to be leveraged in attracting this type of development to the Study Area.



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Additional Initiatives

A community's competitiveness is an increasingly large part of economic development. It really depends on the capacity of a community to adjust to continual change and its leadership's ability to form partnerships. The Study Area is facing a situation many communities have faced before, and many more will in the future. Once the analysis is done, the next step is figuring out how to make it all actually happen. Our general recommendations for economic development include the following:

Spearhead Forsyth area economic development campaign. Undertake partnership efforts with the City of Cumming and the Cumming/Forsyth Chamber of Commerce to pool resources and maximize exposure of the area as a whole in terms of economic development. The North Fulton Chamber of Commerce and the Dawson County Chamber of Commerce are other potential partners. If good things happen in the greater area, everyone can benefit. Business is not conducted with respect to city limits or county lines necessarily; economic development has to have a clear direction and cannot be successful without strong collaboration.

Adopt proactive economic development stance. Move from reactive position of accepting what development or business comes in, and move towards actively recruiting and attracting what types of development and businesses you would like. Once a plan and vision is set, it becomes an issue of pounding the pavement and moving Forsyth County to the next level.

Develop development package of information for Study Area. Multiple packages should be developed for different targets, such as relocating and expanding businesses, developers and real estate brokers. This package should have up-to-date information related to competitive advantages, and answer the question, "Why Forsyth?" All development activity and economic impacts within the County should be tracked and benchmarked as part of this effort. Developing this type of information for multiple areas within the County would be advisable.

Develop objective process and incentives package for quality development. Lending stability to the County's development process attracts developers since they can clearly understand the system. Set your standards high, and avoid making case by case decisions. Develop an objective system that produces only the quality development you desire, using a mechanism such as a checklist with standards and criteria for approval.

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Review zoning requirements. These requirements should be reviewed to ensure that the types of development desired in the area are not being prohibited or inadvertently discouraged. Consider adopting incentives within the Unified Development Code to encourage developers to go in the direction of the vision set out by the County.

Leverage tourism opportunities. Forsyth is in a unique position with access to Lake Lanier and close proximity to the North Georgia Mountains. The Study Area should become a place that people could use as “base camp” to conduct day trips to North Georgia Mountains, and even outlet shopping in Dawsonville. At a minimum, an attraction campaign should be launched to get people traveling for these purposes on GA 400 to exit into the Study Area for something other than gas.



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Recommendations: Land Use, Urban Design, and Zoning

The following Land Use Plan and Zoning recommendations (including Urban Design Guidelines and Land Use / Zoning strategy) successfully implement the Development Plan through a County regulatory process. It is critical that the Land Use and Zoning recommendations be considered earnestly in order to enable for the full implementation of the plan. Most of these changes are in response to the conclusions drawn from the market study and through the community participation process. Without implementation of these suggestions, the vision articulated by the community will be difficult to achieve and the existing type of development patterns will continue.

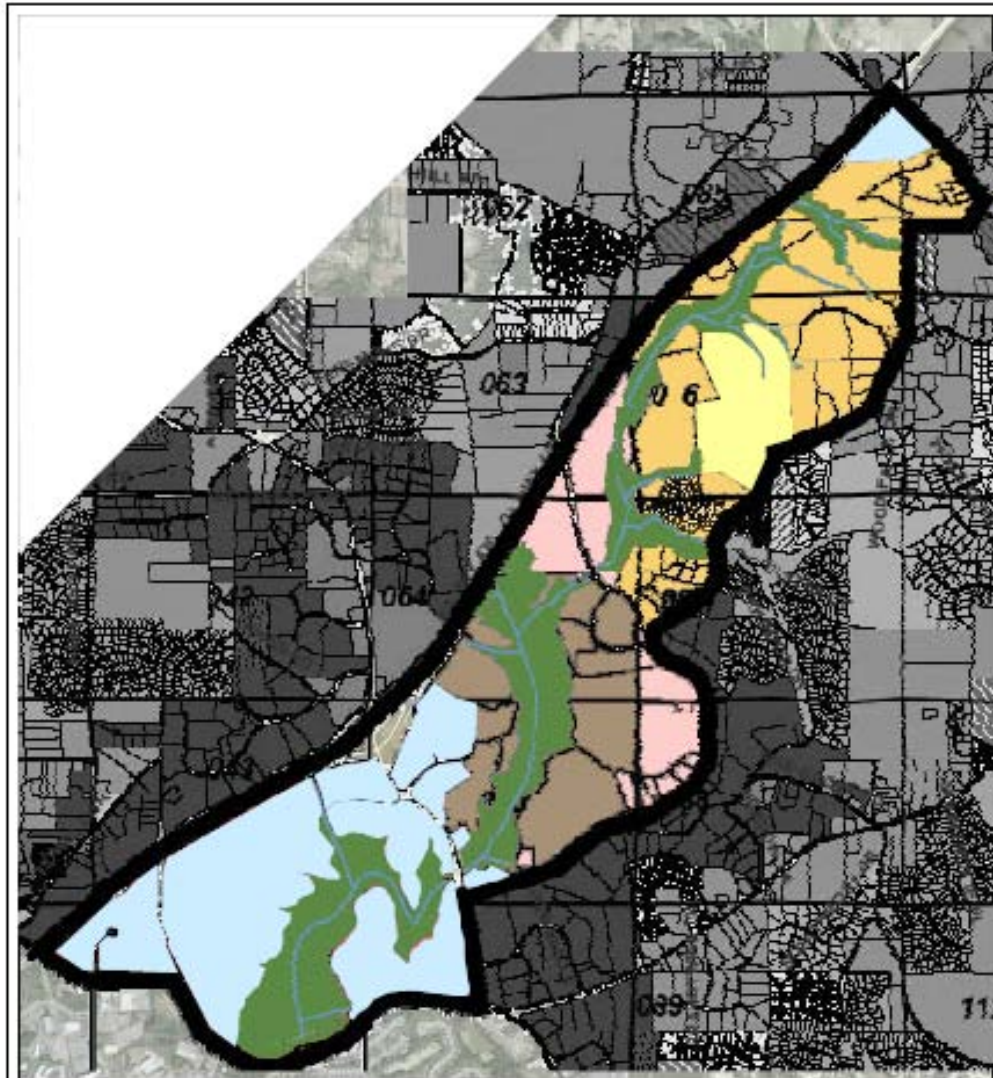
Future Land Use Plan

The Development Plan outlined by this document is implemented by the following Land Use Plan. In addition, the Land Use Goals and Policies as outlined in the Comprehensive Plan and the McFarland Subarea Report are embodied in this Development Plan and subsequent Land Use Plan. This Land Use Plan is to be adopted by Forsyth County and will replace the existing Future Land Use Plan. By adopting this Land Use Plan and considering an alternative zoning mechanism, the LCI Development Plan can be fully implemented as it is envisioned as a result of this document.



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Future Land Use Map 2 of 2

Future Land Use Plan (Recommended)

- Medium Density Residential
- Low Density Residential
- High Density Residential
- Neighborhood Commercial
- Activity Center
- Conservation

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Future Land Use Plan Classifications

Conservation Classification

Throughout the Study Area, Big Creek forms a natural preservation system that reaches into each district and subarea. By conserving this natural open space and adding nature trails and paths, the open space will serve as a unique open space asset and opportunity for the Development Plan area. The open space connects all subareas together and will make an attractive and viable transportation option for the users of the area.

Low Density Residential Classification

This category is intended for single-family detached residential dwellings typically on septic sewer systems. Minimum lot size is .68 acres; overall density is up to 1.5 units per acre. This designation applies only to the Single-Family/Estates subarea around the Kitfox Airstrip.

Medium Density Residential Classification

The Development Plan preserves all existing Single-Family detached residential areas as such under this Land Use classification. It is important for the area to preserve this Single-Family area as it will afford a viable detached residential option for families to live. This will also keep the overall residential density of the area down and thusly the traffic levels of the area can also be kept from being a negative factor for the area. The subarea classified as Medium Density Residential is:

- Single-Family/Estates (partial)

High Density Residential Classification

This area lies between the GA 400, Big Creek and McFarland Road. Higher density residential uses here will afford a variety of single-family attached housing options to the area. Residential options will include apartments and condos that are located along Big Creek away from GA 400 and the commercial/office located between GA 400 and high density residential.



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The subareas designated as High Density Residential Land Use classification include:

- Shiloh Enclave
- McFarland/Ronald Reagan Enclave

Neighborhood Commercial Land Use Classification

The Shiloh Road Corridor constitutes the Neighborhood Commercial district of the area. This corridor serves as a primary connector to the single-family neighborhoods within the Development Plan and as such they offer the best opportunity for providing neighborhood goods and services. These areas serve as an appropriate transitional land use between the higher intensity Activity Center and Multi-Family designations and the lower density Single-Family designations.

The Neighborhood Commercial Land Use Classification areas of the Development Plan are:

- Shiloh Corridor
- Ronald Reagan/Shiloh Road Node

Activity Center Land Use Classification

The area defined by McFarland Road to the north, McGinnis Ferry Road to the south and GA 400 to the west is targeted for a mixed-use community with a variety of uses, retail, offices, variety of housing types and hotel/conference. This will be the mixed-use center of the area and fits the definition of the Activity Center Land Use classification which allows for both residential and non-residential uses. The McGinnis Ferry Node, the McFarland/Ronald Reagan Enclave and the Lanier Tech/Majors Road Node are also Activity Center Land Use classification to ensure a mixture of uses at the edges of the single-family neighborhoods and along these key corridors.

The Development Plan areas that are included in the Activity Center Land Use Classification are identified as follows:

- Jamestown Mixed-Use Development
- McFarland Corridor
- McFarland/Ronald Reagan Enclave
- McGinnis Ferry Node
- Lanier Tech/Majors Road Node

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Urban Design Overlay and Zoning

The following recommendations outline the regulatory mechanisms that must be adopted to ensure the Development Plan is put into action. The proposed strategy utilizes a two-step approach. The first step is the creation of an Urban Design Overlay District to control urban design elements and built aesthetics to achieve an overarching look to the area. The existing zoning is to initially remain unchanged and an overlaying Urban Design Overlay District created to govern urban design elements including building form, sidewalks, parking design, landscaping, building articulation, site design, the relationship of the building to the street and other similar kinds of urban design components. The second step of the zoning strategy is to offer incentives for applicants to rezone to a zoning district that matches the suggested future land use classifications as defined by the Recommended Future Land Use Map.

Urban Design Overlay District

The purpose of the Urban Design Overlay District is control the critical elements of the built form that affect the aesthetics of the area. The overlay district will ensure a consistency in the elements of design on new development while allowing for the same uses, densities and dimensions of the existing underlying zoning.

By implementing a detailed Urban Design Overlay District, new development in the area including those fronting Ronald Reagan Boulevard, McFarland Road or the proposed Town Center area will conform to the design standards regulations providing a quality and predictable visual appearance.

The Urban Design Overlay District for the area would ensure that the specific urban design recommendations made in this plan would be enacted uniformly across the Study Area, regardless of land use designation or zoning district. These elements must be common throughout the Study Area to ensure a unified character and to further promote the establishment of a truly distinct "place". These controls would overlay the underlying zoning and land uses and would implement a series of design controls to create a unique destination in South Forsyth County.



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The overlay language as adopted would be entitled "Urban Design Overlay District" and could be established so as to allow its use in other parts of the County where they may also be needed in the future. In this way, the quality building form regulations of this overlay could be used in other future growth areas of the County to guarantee the same quality building forms would be produced in those areas as well.

SIDEWALKS

Sidewalks are an important of establishing connectivity and pedestrian vitality. The sidewalk environment should be pleasant and extensive to ensure for adequate connections throughout the area. The following elements must be included in the new zoning overlay controls:

- Public sidewalks located along both sides of all streets.
- Local streets should have a sidewalk clear zone of at least 5 feet in width and busier streets should be at least 8 feet in width.
- Landscape zones of at least 5 feet in width, located between the curb and the sidewalk.
 - Include street trees, street lights, benches, planters, trash receptacles, bicycle parking racks and other street furniture, pedestrian lights, landscaping, grass or additional pavement or other similar elements.
 - Sidewalk clear zone should be unobstructed for a height of 8 feet.
 - Utilities placed underground.
 - Street trees on every street and planting on-center and equidistant from other street



A sidewalk clear zone allows pedestrians to safely walk to and from uses.



Utilities are all located underground and street trees are spaced at equal intervals.

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furniture.

- Planted trees should be planted at a caliper and planting area dimension that allows for the trees to grow in a healthy and protected manner.
- Street tree species should stay consistent by blocks to ensure a consistency of look and aesthetics.
- Sidewalks should be provided for all uses on all streets, regardless of the development size or type.
- Sidewalks in parking lots and facilities are needed to ensure safe pedestrian access to and from parking areas and adjacent uses and access routes.



The landscape zone allows for the placement of trees, plantings and other street furniture.

RELATIONSHIP OF THE BUILDING TO THE STREET

The area between building facades and the street is critically important in establishing the design of a community. This area must be scaled and designed to appeal to the pedestrian while also maintaining a safe environment for automobiles. The following elements ought to be addressed through the zoning overlay tool for this area:

- The area between any building façade or parking area and the street should not be used for parking or automotive uses.
- For non-residential uses, this area should be used for fountains, pedestrian furniture, public art and similar elements.
- For residential uses, this area should be used



The area between the building and the street is for active uses and not for automobiles.

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for terraces, porches, stoops and walkways.

- Delineation of building floors at the third story above sidewalk level and lower should be executed through windows, belt courses, cornice lines or similar architectural detailing.



- The primary pedestrian access to all sidewalk level uses and business establishments with public or private street frontage should face the street and be accessible to the sidewalk for all uses.

- Street addresses should be clearly visible from the public sidewalk.

The primary entrance for all uses is adjacent to and accessible to the public sidewalk.

- No barbed wire, razor wire, chain link fence or similar elements should be visible from any public plaza, ground level or sidewalk level outdoor dining area or public right-of-way.

- Retail areas should be defined by storefronts containing high amounts of window fenestration and store entrances.

- Retail should have a minimum floor-to-ceiling height of 18 feet on the first floor.



Retail uses are designed with maximum fenestration to delineate storefront character.

- The length of retail façade without intervening fenestration or entryway should not exceed 20 feet.

- Retail fenestration should be provided for a minimum of 65% of the length of all street frontages.

- Retail fenestration should not utilize painted glass, reflective glass or other similarly treated or opaque windows.

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SITE DESIGN

How each site is developed is important to the total semblance of the place. The use of pedestrian-friendly streets to form a community-sized block structure and subdivision of lots within that block all setup the framework for successful place-making.

- Single-family neighborhoods should have multiple points of ingress and egress to establish travel options to reduce traffic congestion.
- Single-family neighborhoods should have narrow streets with sidewalk bulb-outs, textured paving, crosswalks and similar traffic calming elements.
- Non-residential developments should be divided by streets into blocks having a maximum length of 600 feet.
- Inter-parcel vehicle access points between all contiguous commercial, office, industrial or multi-family residential tracts should be provided.
- Cul-de-sacs should only terminate streets that have a maximum of 250 total linear feet of un-intersected street frontage.
- Pedestrian ways should be utilized to connect internally within larger development projects.
- Drive-through service windows and drive-in facilities should not be located between a building and the street.
- Gasoline fuel dispenser structures and associated vehicular services such as air pumps and car washes should not be located between a building and the street.



Drive-through services and gasoline dispensers are not located between the sidewalk and the street.

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- All dumpsters shall be enclosed with a material similar to the material on the outside of the main building.
- Dumpsters should be placed in the rear yard and should be buffered from adjacent uses.
- Loading dock entrances for non-residential uses should be screened so that loading docks and related activity are not visible from the public right-of-way.



Building heights, facades and massing should be designed to offset all such elements and details.

- Mixed-use buildings should reduce perceived height and bulk by dividing the building mass into smaller scale components. Building walls should utilize offsets, such as projections, recesses, and changes in floor level, to add architectural interest and variety, and to relieve the negative visual effect of a simple long wall.
- Mixed-use buildings should have variation in the roofline of buildings and offsets in pitched roofs and gables shall be required. Parapets in building masses should be varied in height and projection and shall use decorative elements such as crown moldings, dental, brick soldier courses, or similar detail.
- Mixed-use buildings should step back after a certain total height to ensure buildings do not tower over streets and adjacent uses.



Bicycle parking is required and is an ideal element for the landscape zone.

PARKING

When areas develop at higher densities, it is important to accommodate the accompanying parking demands in ways that do not disrupt the fabric of a healthy community.

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Pedestrian-orientation, quality building design and balanced mixed-uses can all be achieved while still meeting the vehicular and parking needs that come with them. They can be designed in a way that minimized the impact of the car and the conflict that can arise with them in densely developed areas.

- Off-street surface parking should not be located between a building and the street.
- Developments should provide compact parking spaces (except single-family developments).
- All non-residential developments should provide bicycle parking within the landscape zone or as close as the closest automobile space.
- Driveways should be a maximum width of 24 feet for two-way entrances and 12 feet for one-way entrances.
- Sidewalk paving materials should continue across any intervening driveway at the same prevailing grade and cross slope as on the adjacent sidewalk clear zone.
- Driveway curb cuts should not be permitted on any street that functions as an arterial street or collector street when access may be provided from a side or rear street.
- Driveways should not be permitted between the sidewalk and a building.
- No more than one curb cut should be permitted for each street frontage.
- A common or joint driveway should be



Sidewalk paving should be carried over across any intervening driveway.



By eliminating curb cuts and driveways, buildings can consolidate together and provide for an attractive environment.

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encouraged between adjacent property owners as a measure for consolidated curb cuts and driveways.

- Parking facilities should have adequate lighting facilities and should be arranged and installed so as not to reflect or cause glare on abutting properties.
- Shared or reduced parking requirements should be encouraged to incentivize the reduction of vehicular congestion.

SIGNAGE

In order to achieve a quality aesthetic in the built environment it is important to control the signage of the uses and to present them in ways that fits in with the area. The signage can be effective in advertising a use while also blending into a building or a block, ensuring that the community as a whole is not negatively effected.

- Signage should be building mounted or allowed on monument signs.
- Signage should use natural elements and should match those of the building for which the signage represents.
- Signage should be limited in size, number and style to fit into the character of the area and so as not to reflect a suburban strip commercial center.



Building signage should be building mounted to ensure for a quality community aesthetic.

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Potential Underlying Zoning Districts

The Urban Design Overlay District alone will not wholly implement the elements of this Development Plan. As envisioned by a majority of community residents, the future development of this area can be altered only with consideration of the underlying zoning classifications. The eventual rezoning of the entire area to a new set of zoning districts is worth considering only if it satisfies the property owners, developers and the Board of Commissioners. Alternatively, an overlay district may address land use in a way that achieves the intent of the plan.

One important consideration in changing land use and zoning is the effect this would have on the taxes paid by property owners. It is conceivable, for example, that the tax burden on a parcel of property could triple when a zoning district is changed to a more intensive use. A case in point would be a single family home that is zoned agricultural that might be rezoned to a residential 4 category. While a developer might see this type of zoning change as beneficial in terms of a project commanding a higher premium, the owner might be forced off the property because of an inability to pay the tax obligation.

Nonetheless, consideration of some method of allowing for rezoning is critical because while the Urban Design Overlay District will govern design aesthetics dealing with the building form or architectural details, it is the underlying zoning that governs substantive elements including uses, densities, yard dimensions, open space controls, height limits and building lot coverage. Existing zoning left alone would not permit the village type of development envisioned in the Town Center area with a mixed residential and commercial environment. Open space elements would not be required at the levels needed to implement the vision of this plan and uses would fail to be connected in a manner needed to ensure a true pedestrian environment as called for in this plan. It is important to consider changing the underlying zoning in order to achieve the kind of place many of the voices heard in the community envisioned throughout this process.

To satisfy the need to implement the land use and zoning recommendations of this study and to find a compromise among competing interests, whenever a rezoning request is made, consideration of the underlying tenets of this effort should be used to guide staff recommendations to the Board of Commissioners. That is, in performing the analysis of the rezoning request, consideration should be

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given to whether or not the proposed development project meets the objectives of creating the community character suggested herein. In addition, the regulations should be enacted such that a zoning request, which does not match the suggested future land use classifications as defined by the Recommended Future Land Use Map, is granted by the Board of Commissioners only if a super majority of the board votes in favor of the rezoning.

Along with the Land Use Plan recommendations, it is critical the Urban Design Overlay District be drafted and adopted within one year of the completion of this study to ensure that all impending development can be controlled through this mechanism. More time can be given to the replacement of the underlying zoning given that the existing area is extremely low-density in nature. The market projections for the area show a much higher density than exists moving into the area and it is expected that this kind of growth and development will be manifested in the area within a 5-year time frame. So the Urban Design Overlay District will ensure quality development whenever it happens and the ultimate replacement of the underlying zoning districts will be implemented in the not-too-distant future to capture the more mature growth that will come in time.

It is a key matter that the Board and County staff stay responsive to the current state of development in the area since the existing properties are characterized as extremely low density and even agricultural in nature. The growth from Fulton County and even the greater Atlanta region will come and that is not in doubt.



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Potential Underlying Zoning Districts

The following are the suggested Potential Underlying Zoning Districts that will enable the County to realize the goals of the study and the attendant economic development of the Study Area:

Agricultural District (A1). At the Public Workshops, the community expressed a desire to preserve the Kitfox Airstrip and surrounding residences. Retaining the zoning of this land as *A1* is a means to accomplish this. As expressed in the Forsyth County Unified Development Code, the Agricultural District "helps to preserve the rural, open space character that exists in many portions of the County."

Single-Family Residential Zoning Districts. The northern end of the Study Area is characterized by larger single-family and estate homes. These residential areas are to be preserved and other vacant land around is proposed for lower density residential uses. This is the Single-Family/Estates subarea.

Multi-Family Residential Zoning District (Residential 6). The Multi-Family Residential Zoning District should be incorporated into some areas of the Shiloh Enclave and McFarland/Ronald Reagan Enclave subareas of the Development Plan. The Shiloh Enclave subarea abuts Big Creek on the western edge. This area should mix high density residential with neighborhood scale retail and small pocket parks in close proximity to the residents. The McFarland/Ronald Reagan Enclave is between the GA 400, Big Creek and McFarland Road. If public/private efforts fail to build on existing office to generate industrial uses, high density residential should be an option. The proposed future uses for this area are high density residential, office and commercial that benefits from good access to GA 400 in close proximity. High density residential to include apartments and condos that are located along Big Creek away from GA 400 and the commercial/office located between GA 400 and high density residential. The details should be worked out to achieve appropriate balance of maximum residential density to support new convenience retail.

Neighborhood Shopping Zoning District. Two distinct areas could form the Neighborhood Shopping Zoning District of the Study Area, Shiloh Corridor and Ronald Reagan/Shiloh Road Node.

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The east side of the Shiloh Road is presently lined with predominantly office/warehouse development. To relate to this use across the street, the west side of Shiloh Road that lies within the Study Area is proposed for neighborhood-oriented mixed-use primarily office/commercial/condos that can offer flexibility in land use and also provide a transition to the residential behind. The small node at the intersection of Ronald Reagan Boulevard and Stoney Point Road is proposed for neighborhood-oriented mixed use node with retail, office and high density residential; townhomes and condos.

The Neighborhood Shopping Zoning District will provide convenient goods and services to the adjacent single-family neighborhoods while also forming an ample buffer between the single-family neighborhoods and the higher density Urban Village and Town Center Zoning Districts.

Urban Village Zoning District. The Urban Village Zoning District provides for a mixed-use environment and an opportunity for higher density commercial and residential development. This district could be situated along the major corridors to provide for adequate densities along higher traffic roads.

The McFarland Corridor is envisioned to support one- or two-story retail along both sides of the street with a well articulated landscape strip. Most of the parking is to be located at the back with a single bay of parking in front of the building facing McFarland Road that offers customers access to pull up in front of the building to make the retail successful. Lanier Tech/Majors Road Node will offer retail services that cater to students and staff. Potential offices or business that can leverage the programs offered by Lanier Tech are also proposed at this node. The availability of land at this node limits the extent of services that can be provided in the short-run.

The McFarland/Ronald Reagan Enclave will provide a true mixed-use environment including high density residential, office and commercial that benefits from good access to GA 400 in close proximity. If public/private efforts fail to build on existing office to generate industrial uses, high density residential should be an option. High density residential to include apartments and condos that are located along Big Creek away from GA 400 and the commercial/office located between GA 400 and high density residential. The McGinnis Ferry Node along the McGinnis Ferry Road and

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adjacent to the Town Center Zoning District mixed-use should be 3-4 stories that can relate to the mid-rise office buildings across the street.

Town Center Zoning District. The Jamestown Mixed-Use Development area (the large tract bound by McFarland Road to the north, McGinnis Ferry Road to the south and GA 400 to the west) is targeted for a community of mixed-uses including retail, offices, variety of housing types, and hotel/conference. The development should integrate Big Creek and provide public access to Big Creek and Greenway connectivity to the Big Creek Trail at various locations. This will be the highest concentration of density of the entire area and will allow for offices, large retailers, a hotel/conference center, apartments, condos and even larger civic uses. The Town Center area would be the central gathering place for the area and would be the ultimate destination for South Forsyth County.

The existing Zoning Districts that come close to meeting the need of this area are the Master Planned Zoning District and the Commercial Business Zoning District. However, the Commercial Business District has certain requirements pertaining to maximum building coverage, maximum building height, minimum front building setback and maximum residential density that render it inadequate for attaining the goals and vision as outlined in this Development Plan for the area designated as the Town Center Zoning District. A new Town Center Zoning District may be necessary to create if the Master Planned Zoning District does not sufficiently allow high intensity of land use.

The following regulations must be added to the proposed Town Center Zoning District:

Minimum lot area for zoning to the district: None

Maximum building coverage (% of lot): 80%

Maximum gross square footage per business establishment: No maximum

Maximum height (number of stories): six-stories

Maximum height (feet): 80

Minimum landscape strip paralleling road frontage (feet): 6

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Minimum front building setback/front yard from dedicated right of way: 20

Minimum buffer along side or rear yard abutting a residential, OR, A1, A2, AG-RES or CONS district (feet): 40

Minimum building setback along side or rear yard abutting a residential, OR, A1, A2, AG-RES or CONS district (feet): 40

Minimum side building setback/side yard (feet): 10 or zero-lot-line

Minimum rear building setback/rear yard (feet): 25 or zero-lot-line

Minimum rear building setback abutting an alley (feet): None

Maximum density, residential uses (units per acre): 15

Minimum heated floor space: 900 square feet

By creating a new Town Center Zoning District, the County will have another tool for creating and enforcing a mixed-use building form that is urban in characteristic. When complimented with the Urban Design Overlay District, the Town Center Zoning District, along with all the other Potential Underlying Zoning Districts of this Development Plan, will create the unique destination and place desired and outlined in this document.



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Recommendations: Transportation

This study confirms that there is sufficient market demand for mixed-use development to support an activity center in the LCI Study Area. The Development Plan concentrates this development in liveable central nodes at intensities and locations deemed appropriate by the community. Adoption of the recommended land use plan and design guidelines will ensure new development is in accordance with the intent of the plan.

Still, regulations and rezoning conditions placed on private developers on an incremental, parcel-by-parcel basis may not produce the complete network of infrastructure necessary to support the community plan. Thus, it is necessary for the government to pro-actively engage in the development process in order to guide and even influence private development. Direct public investment (in right-of-way and multi-modal transportation facilities) may be the most important tool the government has to ensure provision of an adequate network of connectivity to support the liveable centers concept. Such investments confirm government dedication to the plan and decrease the risks of uncertainty associated with private investment. Further, in order to protect public investment, policies must be adopted to preserve the functionality of the network through access management.

Metro-Atlanta has come to realize that transportation is not an independent element of regional planning. There is an intrinsic relationship between transportation facilities and the Land Uses they connect. Though Urban Design addresses aesthetic elements, good design is critical to the evolution of a viable pedestrian environment and informs transportation planning of pedestrian facilities.

Thus, many of the principles, guidelines, and recommendations previously mentioned with regard to the Development Plan, Land Use, and Urban Design are in fact transportation-related. The conceptual alignments and cross sections of Study Area streets were presented in the Community Development Plan. Urban Design guidelines addressed the provision and design of sidewalks and parking facilities. This section specifically addresses access management, pedestrian design, and Ronald Reagan Boulevard before presenting the recommended capital improvement projects and transportation-related initiatives necessary to support the Development Plan.

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Access Management and Pedestrian Design

This plan seeks to steer the Study Area toward a more effective transportation network as well as a more pedestrian-friendly environment. The design principles underlying the transportation recommendations of this study fall into two categories: access management principles and pedestrian design principles.

Access Management (AM) is the systematic control of the location, spacing, design, and operation of driveways, medians, auxiliary lanes, intersections, traffic signals, and street connections. Effective AM can increase public safety, extend the life of major roadways, reduce traffic congestion, support alternative transportation modes, and improve the appearance and quality of the built environment.

Following are ten access management principles promoted by the Transportation Research Board:

1. *Provide a Specialized Roadway System.* Manage each facility according to its function.
2. *Limit Direct Access to Major Roadways.* Higher volumes require greater access control.
3. *Promote Intersection Hierarchy.* Provide appropriate transitions between road classifications.
4. *Locate Signals to Favor Through Movements.* Space intersections for future signalization and coordination.
5. *Preserve the Functional Area of Intersections.* Carefully regulate access near intersections.
6. *Limit the Number of Conflict Points.* Simplify the driving environment by minimizing opportunities for conflict.
7. *Separate Conflict Areas.* Curb cuts, crosswalks, median breaks, and intersections must be adequately spaced to give motorists time to perceive and react to a potential conflict.
8. *Remove Turning Vehicles from Through Traffic Lanes.* Provide adequate auxiliary lanes.
9. *Use Non-traversable Medians to Manage Left-turn Movements.* Minimizing left-turns with raised medians has been proven to improve safety.
10. *Provide a Supporting Street and Circulation System.* Interconnected streets accommodate development, unify property access, support transportation alternatives, and provide alternate routes.

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Specific roadway design standards which are recommended to incorporate these principles include:

- All roadways with 4 or more travel lanes shall be divided by a 19-foot (minimum) raised median.
- For roads classified as major arterials or freeways, travel lanes shall be 12-foot wide; for roads classified as minor arterials or collectors, travel lanes shall be 11-foot wide.
- Where arterials and collectors pass through activity centers, design speeds and posted speeds shall not exceed 35 miles per hour (mph).
- Median crossovers shall be spaced at 1320-foot (optimal), 1000-foot (minimum), in order to meet minimum future signal spacing requirements.
- For design speeds of 45 mph or greater, curb cuts and driveways shall be spaced at 230-foot (minimum) from adjacent rights of way or other curb cuts.
- For design speeds of 35 mph or less, curb cuts and driveways shall be spaced at 150-foot (minimum) from adjacent rights of way or other curb cuts.

Following are seven principles of pedestrian facility design:

1. *Share the Road.* A major shift in mentality for designers as well as users of local streets is toward a pedestrian dominated streetscape where vehicles truly give right-of-way to bicyclists and pedestrians.
2. *Share the Right-of-way.* A popular trend in provision of bike/ped facilities is to segregate pedestrian travel from motorized travel by providing right-of-way or easements dedicated to shared use paths (as in the case of a multi-use trail in a greenway). Though such facilities are highly recommended and play a key role in accommodating both bicycles and pedestrians, they do not sufficiently accommodate non-recreational trips. Consider that such trails are often isolated to areas where development is severely restricted, as in floodplains. Users must inevitably leave the trail to reach a destination. Thus, a complete pedestrian circulation network must include not only trails in dedicated easements but a complete network of sidewalks, paths, and walkways within the right-of-way to accommodate complete pedestrian trips.
3. *Reduce Conflict Points with Motorized Traffic.* This principle is also a key to access management and may be accomplished by restricting the number, width, and spacing of intersections, curb cuts, and driveways as previously mentioned.

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4. *Provide Pedestrian Refuge.* Inevitably, pedestrians must cross the street; thus, signalized crosswalks and signed mid-block crossings play a critical role in safe pedestrian circulation. Local standards should address the maximum width of travel lanes a pedestrian should be expected to cross before reaching a refuge zone (such as a raised median or island).
5. *Buffer Pedestrians from Motorized Traffic.* An important detail in walkway provision is provision of an adequate buffer from motorized traffic. Curb and gutter offers a necessary, first barrier. Then, adequate spacing of a sidewalk from the back of curb is necessary and may take the form of a planting strip which accommodates street trees as well as utilities.
6. *Provide Adequate Bicycle/Pedestrian Travel Zone Widths.* Such facilities should be designed to meet not just minimum, but optimal standards. Such standards and/or guidelines have been adopted at the county, region (ARC), and state (GDOT) levels and vary according to the design context, land uses, and anticipated bicycle/pedestrian traffic.
7. *Accommodate Pedestrian Connections to Private Development.* Allow private development to not only tie into public sidewalks but also to expand the width of sidewalks in the right-of-way to support pedestrian-oriented street frontage where identified in the Development Plan.

Supplemental to the urban design guidelines, the following specific design standards are recommended to incorporate these principles:

- Add “share the road” signage along Study Area arterials and collectors.
- Provide sidewalks and/or multi-use trails along both sides of all collectors and arterials. For local streets, require sidewalks unless developers provide detailed traffic analyses which prove that traffic volumes and speeds will be low enough so as not to deter pedestrian travel within the roadway.
- Where said sidewalks, paths, and trails run adjacent to roadways, provide planting strips (5-foot minimum) to buffer pedestrians from motorized travel as well as to accommodate street trees and utilities.
- Where pedestrians will be required to cross more than three traffic lanes, provide refuge on raised and landscaped islands, bulb-outs, and medians.
- Design pedestrian facilities according to optimal standards including: 6-foot sidewalks; 12-foot multi-use trails.

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Ronald Reagan Boulevard

If the Study Area is to emerge as a livable activity center, the Ronald Reagan Boulevard may be the catalyst project which will spawn it. Its conceptual alignment is clearly the prominent transportation feature in the future land use plan for the area, extending over 4.5 miles from Majors Road southward through intersections with Shiloh Road and McFarland Road to provide the fourth leg of the intersection with McGinnis Ferry Road and Union Hill Road.

Though the design is officially conceptual, construction is already underway! Private developers are actively grading the northern segment of the roadway (between Shiloh Road and Majors Road) in accordance with an electronic, conceptual plan being hosted by a consulting firm. The segment between Union Hill Road and McFarland Road is basically a negotiation/conversation between the county and three independent developers. Because private interests arguably have the most to gain from the boulevard construction, informal county policy has been to approve rezoning requests with conditions to dedicate a 100-foot right-of-way corridor and in some cases grade and/or construct the road through the property. Because these processes have occurred simultaneously to this study, it is difficult to report the status of the roadway.

The primary benefit of the County policy is that private money will help fund the road. Consider however, that the roadway offers benefits well beyond parcel-level interests. The roadway has been sanctioned by the County (in the Comprehensive Plan and Future Land Use Map) as well as the ARC (in the Regional Development Plan). Consider, however, that these public bodies are sanctioning the concept of the complete roadway, not segments of it. It will be extremely unfortunate for the community if the roadway never extends the entire length as originally conceived.

The two key dimensions of the roadway concept are the alignment and the typical cross section.

Ronald Reagan Boulevard Alignment

With regards to alignment, the only segment of the roadway which is more or less determined is the northern section between Majors Road and Shiloh Road.



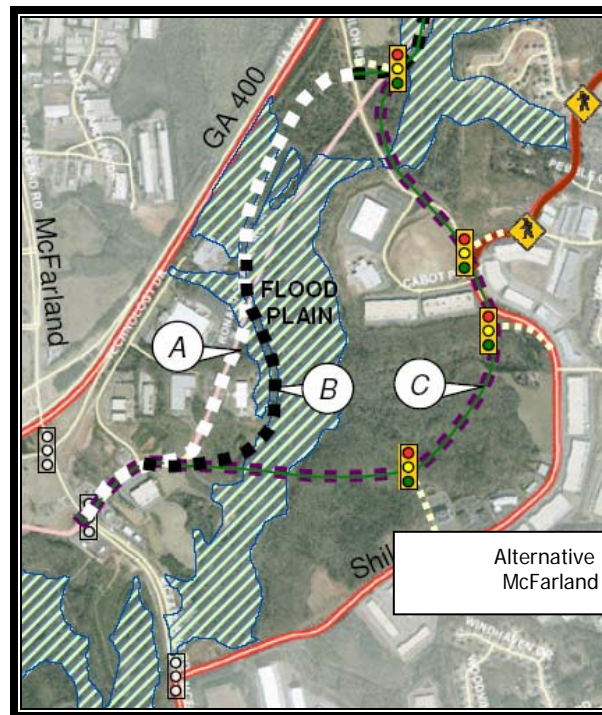
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The alignment between Union Hill Road and McFarland Road may have the biggest impact on the future function of the roadway. An early concept showed the roadway terminating at Union Hill Road just south of the GA 400 bridge. Later concepts suggest termination at the Union Hill Road / Windward Concourse intersection or, as most recently conceived, at the McGinnis Ferry Road / Union Hill Road intersection. If Ronald Reagan Boulevard dead-ends at the McGinnis Ferry Road / Union Hill Road intersection, it will be the straight shot east from a future interchange at GA 400 and McGinnis Ferry Road, and will definitely be required to function as an arterial if and when the interchange is constructed. Alternately, if the boulevard connects Windward Concourse to Bluegrass Valley Parkway (as early concepts portrayed) it would operate more as a collector.

The roadway alignment between McFarland Road and Shiloh Road has not been formalized. Early concepts called for using the entire length of existing Bluegrass Valley Parkway right-of-way before traversing the Big Creek floodplain parallel to GA 400 (option "A" in the following map). The Development Plan recommends and incorporates this alignment.



Alternative Boulevard Alignments
McFarland Road to Shiloh Road

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Besides the obvious environmental costs associated with the floodplain, the primary problem with alignment option "A" was opposition from the owners of industrial property through which it would run.

For this reason apparently, the alignment was shifted to completely bypass an industrial parcel to the east (option "B" in the map). This new, though still conceptual, alignment introduces a host of problems including:

- environmental costs associated with running the roadway in the Big Creek floodplain,
- construction costs of encroaching on the creek bed topography,
- foregone opportunity to develop the eastern frontage of the road, and
- direct conflict with the Big Creek Greenway Trail alignment (in design).

Here, a new alternative alignment is introduced for the county's consideration (option "C" in the map). The boulevard would depart from the Bluegrass Valley Parkway right-of-way just north of McFarland Road and turn due east to cross Big Creek at its narrowest section in this area. This alignment minimizes the construction and environmental costs associated with the floodplain. Then the road would gently curve to the north through undeveloped land and jump onto the Shiloh Road right-of-way through the intersections with Shiloh Road East and Cabot Parkway. Finally, the road would cross Bagley Creek at its narrowest section in the vicinity of the existing Shiloh Road bridge and tie in to the agreed upon alignment to the north end of the Study Area. Alignment "C" has the following advantages:

- avoiding the aforementioned industrial parcel ;
- enabling development of both west and east frontages of the boulevard ;
- minimizing the conflict with the Big Creek Greenway Trail ; and
- reducing the environmental impact on the Big Creek floodplain.

Ronald Reagan Boulevard Cross Section

As presented in the community development plan, the boulevard cross section will consist of 2 lanes each direction divided by a raised median. A sidewalk and multi-use trail are proposed to share the right-of-way. In order to adequately accommodate these functions in 100 feet of right-of-way while

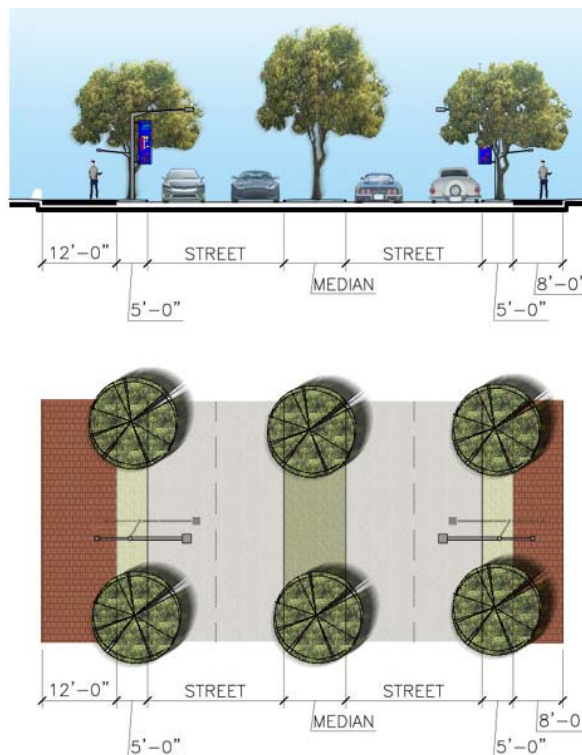
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holding to the Urban Design Guidelines and the Development Plan, the following dimensions are recommended:

- the median should be 19-ft, raised and landscaped (so as to accommodate an 11-ft left- or u-turn lane and 8-ft raised median where approaching intersections) ;
- 2 travel lanes each direction, each should be 11-ft wide ;
- standard curb and gutter (2.5-ft) and a 5-ft (minimum) landscaped planting strip will buffer pedestrian facilities from the roadway ;
- a 6-ft (minimum), 8-ft (ideal) sidewalk should be provided on the southbound shoulder ;
- a 12-ft (minimum), 15-ft (ideal) multi-use trail should be provided on the northbound shoulder; and
- 2-ft clear zones will provide outside shoulders for the pedestrian facilities and account for the remainder of right-of-way.



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Recommended Transportation Projects (TP)

This section details the recommended capital improvement projects involving new or improved transportation facilities.

Many of the projects recommended by this study were derived from existing plans. Some already have dedicated funds and have been programmed (in the County's Short Term Work Program or the Regional Transportation Improvement Program). Others have merely been proposed by a variety of public and private interests (including the County's Bicycle and Pedestrian Plan adopted as part of the Comprehensive Plan). Still others have evolved from this planning process.

Major transportation improvement projects identified prior to this study include:

- A new major roadway (namely, Ronald Reagan Boulevard) ;
- A new multi-use bike/ped facility (the Big Creek Greenway Trail) ;
- Major roadways improvements (including McFarland Road to 6-lanes and McGinnis Ferry Road and Union Hill Road to 4-lanes); and
- New local streets serving new residential development in the north end of the Study Area.

Other transportation plans previously proposed include:

- A new multi-use bike/ped facility (parallel to GA 400) ;
- New sidewalks along Shiloh Road and Shiloh Road East ;
- A new major interchange (at McGinnis Ferry Road and GA 400) ;
- An Express Bus Park & Ride Lot near the GA 400 / McFarland Road Interchange ; and
- New local streets serving a new mixed-use activity center (south of McFarland Road).

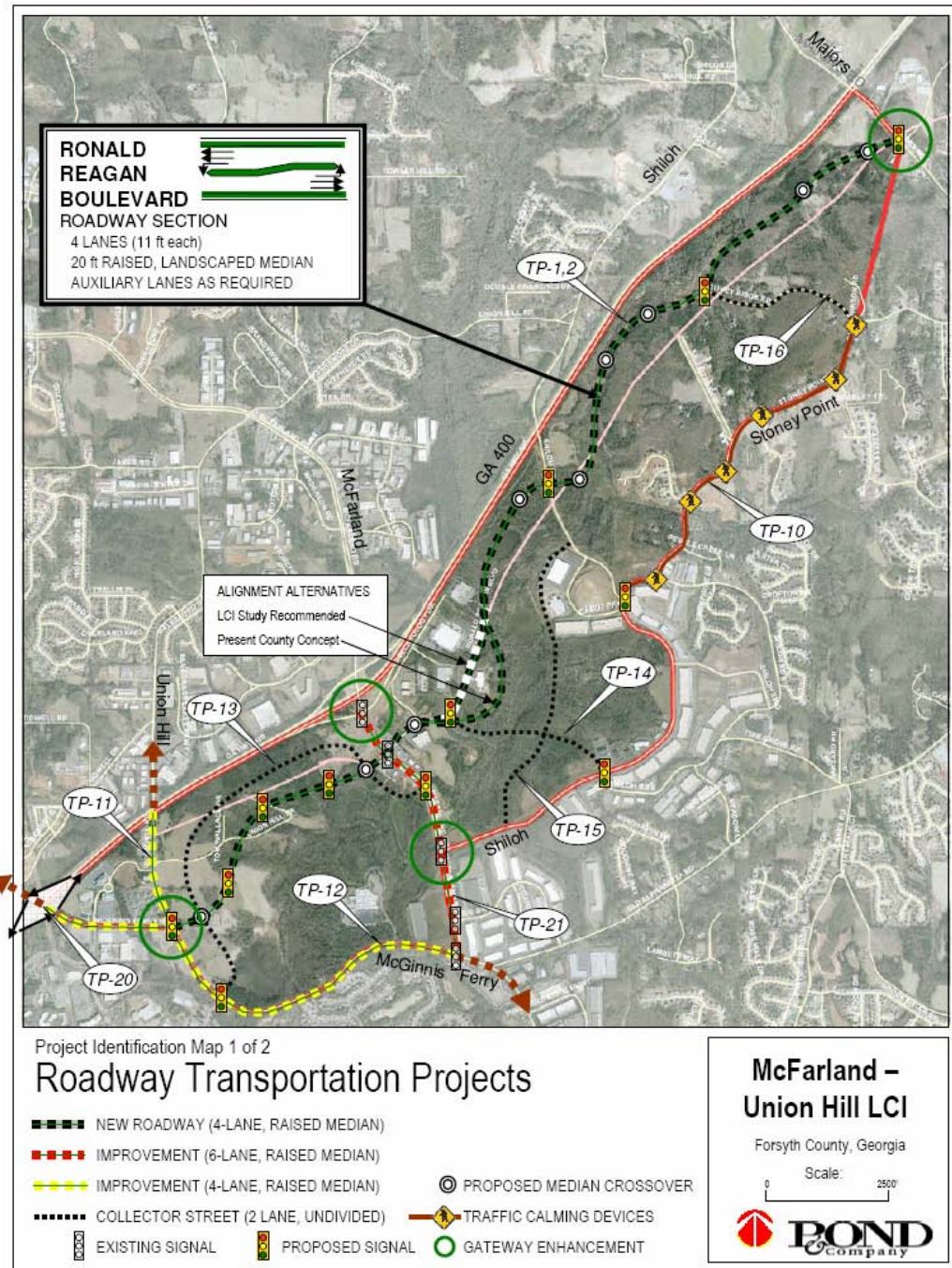
Project recommendations which evolved from this study process include:

- Traffic calming measures on Stoney Point Road ;
- New or enhanced pedestrian facilities in all existing and future, dedicated right-of-way ; and
- New local, pedestrian-friendly streets to support the Development Plan.

Two maps displaying the roadway and alternative transportation projects follow.

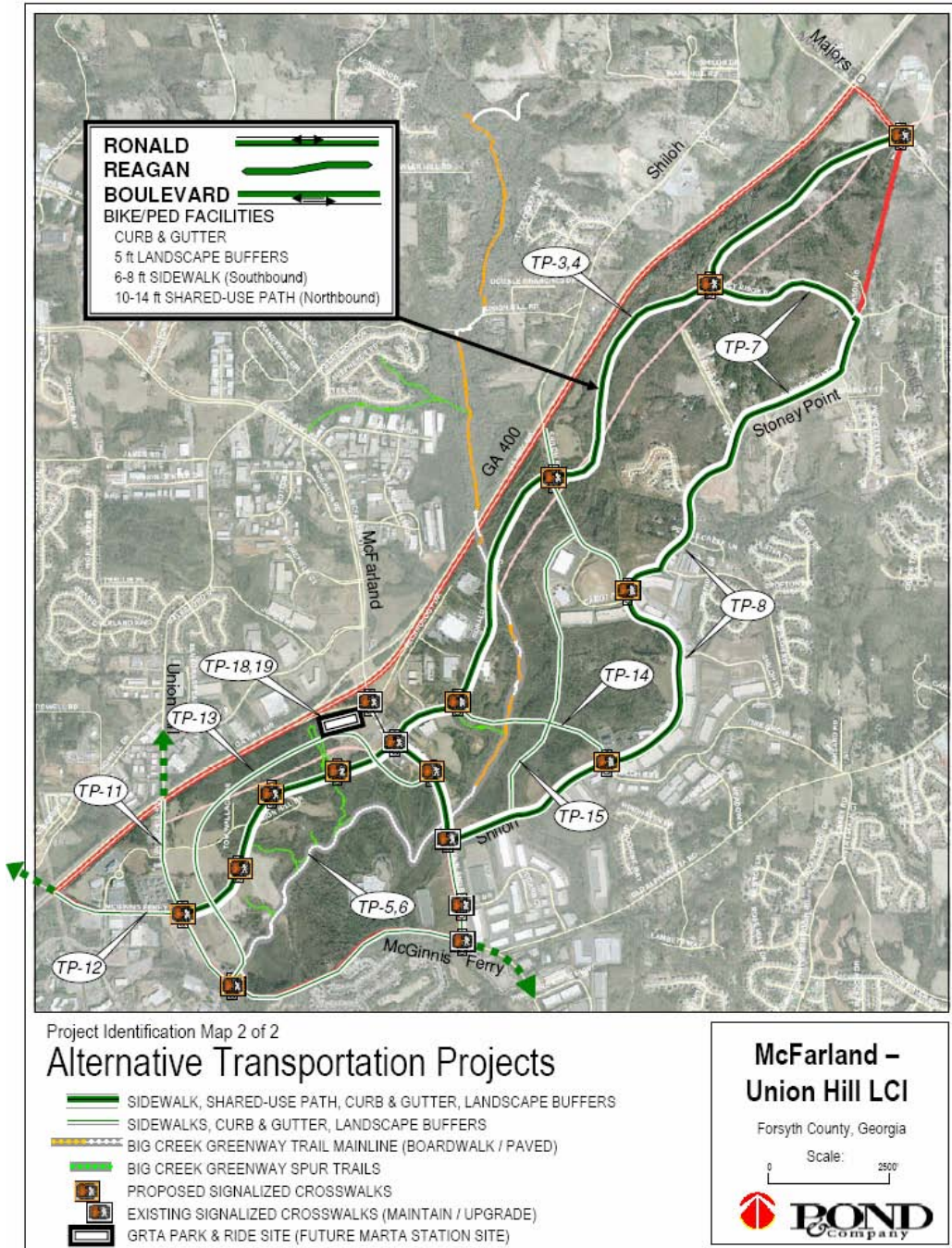
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Following is a description of each of the recommended capital improvement projects. These are necessary improvements to the transportation infrastructure to support the community's livable center plan for the McFarland - Stoney Point Study Area. They identify the locations of proposed transportation improvement projects for the Study Area.

TP-1. Ronald Reagan Boulevard Extension southward from Majors Road to McGinnis Ferry Road. Two X 11-foot lanes each direction, divided by 20-foot raised median.

This new roadway project is a critical piece to development of the area. The roadway will ultimately function as a minor arterial. As such, its primary role will be mobility of through traffic seeking an alternative to GA 400. Still, it will serve an important role in providing regulated access to new development.

TP-2. Ronald Reagan Boulevard: Landscape 20-foot raised median.

Landscaping the median along Ronald Reagan Boulevard has been pulled out as a separate project from the boulevard due to its variable funding sources. This is primarily a streetscape enhancement.

TP-3. Ronald Reagan Boulevard Multi-Use Path: 14-foot wide (12-foot minimum). Landscape planting strip: 10-foot buffer from curb (5-foot minimum). Located on the southbound side of the Boulevard.

This recommendation stems from the County's Bicycle and Pedestrian Plan. The landscaping strip is necessary to buffer trail use from travel lanes.

TP-4. Ronald Reagan Boulevard Sidewalk: 8-foot wide (6-foot minimum). Landscape planting strip: 5-foot buffer from curb. Located on the northbound side of the Boulevard.

Sidewalks on this side of the boulevard are necessary for pedestrian safety and circulation.

TP-5. Big Creek Greenway: Multi-Use mainline trail from McGinnis Ferry Road to GA 400. Includes parking and a spur trail at McFarland Road.

This project will serve as the primary alternate transportation route through the southern portion of the Study Area as well as providing for linear recreation. The trail will cross under the GA 400 bridge

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to tie-in with the County's Big Creek Greenway Trail project continuing to the north. This path will provide pedestrian and bicycles with the only E-W connection across GA 400 completely segregated from automobile traffic.

TP-6. Spur Trails linking Development with the Big Creek Greenway Mainline Trail.

The County should anticipate interest in tying private spur trails into the mainline trail. Though costs shall be incurred by property owners/developers, the County should prepare guidelines and regulate construction of the trails, as they will encroach on Big Creek floodplain.

TP-7. McFarland / Shiloh / Stoney Point / Stoney Point Ridge Multi-Use Trail Loop.

This project completes a viable multi-use trail network to serve alternate modes of travel in the area. The shoulders of existing roads would be converted to a new section including standard curb and gutter, a 5-foot minimum width landscape strip, a 12-foot minimum width multi-use path, and a 2-foot unpaved shoulder / clear zone.

TP-8. McFarland / Shiloh / Stoney Point / Stoney Point Ridge Sidewalks

This project accommodates pedestrian trips, improving the existing shoulders to a new section including standard curb and gutter, a 5-foot minimum width landscape strip, a 6-foot minimum width sidewalk, and a 2-foot unpaved shoulder / clear zone.

TP-9. Internal pedestrian system: sidewalks along new development streets.

New roads within private development should accommodate pedestrian travel not only to but through the site. Thus development regulations should require new pedestrian facilities and connectivity.

TP-10. Traffic Calming: Narrower Streets, Curb Extensions (Bulb-outs and/or Chicanes) (5 locations)

Shiloh Road and Stoney Point Roads were identified as hosting high speeds and high volumes of cut through traffic. It is anticipated that some of this traffic will be relieved by the Ronald Reagan Boulevard project; thus, increasing capacity along these collector streets is not recommended. Alternately, these rights of way should be reclaimed as local streets which serve pedestrian and bicycle trips. The aforementioned path and sidewalk projects will not only provide the facilities

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necessary for alternate modes, but also improve the sight distance for motorized vehicles. However, for the pedestrian environment to be truly viable, traffic must be calmed. Thus, where narrowing an entire length of road may be impractical, bulb-outs and chicanes are proposed along these routes at intersections and at selective straight-away locations, respectively, to deter high speed travel.

TP-11. Union Hill Road Improvement

Already in the Regional TIP, this study confirms the need for this project.

TP-12. McGinnis Ferry Road Improvement

Also already in the Regional TIP, this study confirms the need for this project.

TP-13. New road parallel to Ronald Reagan Boulevard between McFarland Road and Union Hill Road

This road provides a pedestrian friendly alternative to Ronald Reagan Boulevard for local trips in this area.

TP-14. New E-W connector: Shiloh Road East extension, Shiloh to Ronald Reagan Boulevard.

This road provides a pedestrian friendly alternative to McFarland Road for local trips in this area.

TP-15. New N-S connector: Ronald Reagan Boulevard alternative east of Big Creek, Shiloh NB to Shiloh SB.

This road provides a pedestrian friendly alternative to Ronald Reagan Boulevard for local trips in this area.

TP-16. New E-W connector: Stoney Ridge Drive Improvement

This road provides an important link between Stoney Point Road and the new Boulevard. New residential developments will utilize this road, and the County should reclaim and maintain it as right-of-way. In the short term, this road as is would make a great multi-use trail in its current state. In the

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long term, this road should be improved to a 2-lane street with sidewalk on the north side and shared-use path on the south side.

TP-17 Internal local roadway network.

LCI-specific Design Standards and connectivity requirements must be placed on new local roads in the area.

TP-18 GRTA Park and Ride Facility.

GRTA has identified this area for a park and ride lot. New development should be required to present a pedestrian plan which addresses this facility.

TP-19 MARTA Transit Station.

In the long term, it is feasible that the GRTA facility may be converted to a MARTA station.

TP-20. GA 400 / McGinnis Ferry Road Interchange.

This project is in early concept phase, but would have a major impact, likely spurring redevelopment of parcels in the immediate vicinity. Thus, this area has not been designated for immediate improvements or redevelopment until the interchange project scope and timeframe are clear.

TP-21. McFarland Road Widening.

Already in the Regional TIP, this study confirms the need for this project, particularly the construction of a raised median and provision of pedestrian facilities.

TP-22. Gateways.

Gateway treatments generally encompass a wide variety of techniques that provide neighborhood identification, such as signs, monuments, landscaping, special paving, narrowed entrances, and other elements. These enhancements help to provide an indication to motorists that they are entering a neighborhood area from an arterial road or other type of street where traffic was moving at higher speeds.



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Other Transportation-Related Initiatives

Following is a description of additional transportation-related initiatives including policies, standards, or guidelines recommended for implementation.

TRI-1. Incorporate Access Management and Pedestrian Design Principles into Zoning Regulations and Urban Design Guidelines.

This action would implement the principles previously discussed.

TRI-2. GA 400 Setback Ordinance.

This ordinance would preserve the right-of-way required in the long term future for construction of transit facilities parallel to GA 400.

TRI-3. Coordinate with Fulton County & GDOT for McGinnis Ferry Road Improvements

Long term plans related to McGinnis Ferry Road (especially the interchange with GA 400) should be coordinated with state, regional, and county interests. A land use and transportation study is recommended to consider the impacts of the interchange on local development and transportation facilities.



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Implementation Plan

The following is a schedule of implementation for the aforementioned transportation projects, transportation-related initiatives, land use initiatives, and economic development initiatives. Emphasis is placed on the short-term (5-year) recommended actions which signify community priorities.



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FIVE YEAR IMPLEMENTATION PLAN

McFarland - Stoney Point LCI

Project ID	Description / Action	Type of Improvement	Engineering Year	Engineering Costs \$	ROW Year	ROW Costs \$	Construction Year	Construction Costs \$	Total Project Costs \$	Responsible Party	Funding Source	Local Source
TP-1	Ronald Reagan Boulevard Corridor: Roadway Extension from Majors Road to McGinnis Ferry Road. Four-lane (11-12 ft) divided by 20 ft raised median.	Roadway: New Facility	UNDERWAY	2,275,000	2006	4,550,000	2006-2008	15,925,000	22,750,000	Forsyth County, Private	Forsyth County, Private	Forsyth County
TP-2	RRBlvd Corridor: Landscape 20 ft raised median.	Streetscape Enhancement	2005-2006	INC	2006	INC	2006-2008	INC	-	Forsyth County	Forsyth County	Forsyth County
TP-3	RRBlvd Corridor: Shared-Use Path. 10-15 ft variable width. Landscape 5-10 ft planting strip buffer from curb.	Pedestrian: New Facility	2005-2006	50,000	2006	INC	2006-2008	2,000,000	2,000,000	Forsyth County	ARC, FC	Forsyth County
TP-4	RRBlvd Corridor: Sidewalk. (6-) 8 ft width. Landscape 5 ft planting strip buffer from curb.	Pedestrian: New Facility	2005-2006	25,000	2006	INC	2006-2008	1,000,000	1,000,000	Forsyth County	ARC, FC	Forsyth County
TP-5	Big Creek Greenway: Shared-Use Mainline Trail, with trail access at McFarland.	Pedestrian: New Facility	UNDERWAY	130,000	2005-2006	225,000	2006-2007	2,580,000	2,935,000	GDOT, FC	GDOT, FC	Forsyth County
TP-6	Spur Trails linking Development with Greenway Mainline Trail	Pedestrian: New Facility	2010	200,000	2011	225,000	2012	3,200,000	3,625,000	Private	Private	Private
TP-7a	McFarland / Shiloh / Stoney Point / Stoney Ridge Multi-Use Trail Loop - <i>Stoney Point is a core team priority</i>	Pedestrian: New Facility	2007	150,000	2008	300,000	2009-2010	3,000,000	3,450,000	Private	ARC, FC	Forsyth County
TP-7b	McFarland / Shiloh / Stoney Point / Stoney Ridge Multi-Use Trail Loop - <i>Stoney Point is a core team priority</i>	Pedestrian: New Facility	2008	150,000	2009	300,000	2009-2010	3,000,000	3,450,000	Private	Private	Private
TP-7c	McFarland / Shiloh / Stoney Point / Stoney Ridge Multi-Use Trail Loop - <i>Stoney Point is a core team priority</i>	Pedestrian: New Facility	2009	150,000	2010	300,000	2009-2010	3,000,000	3,450,000	Private	Private	Private
TP-8a	McFarland / Shiloh / Stoney Point / Stoney Ridge Sidewalks - <i>Stoney Point is a core team priority</i>	Pedestrian: New Facility	2007	100,000	2008	200,000	2009-2010	2,000,000	2,300,000	Private	Private	Private
TP-8b	McFarland / Shiloh / Stoney Point / Stoney Ridge Sidewalks - <i>Stoney Point is a core team priority</i> 11-8-05	Pedestrian: New Facility	2008	100,000	2009	200,000	2010	2,000,000	2,300,000	Private	Private	Private
TP-8c	McFarland / Shiloh / Stoney Point / Stoney Ridge Sidewalks - <i>Stoney Point is a core team priority</i> 11-8-05	Pedestrian: New Facility	2009	100,000	2010	250,000	2010	2,000,000	2,350,000	Private	Private	Private



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Project ID	Description / Action	Type of Improvement	Engineering Year	Engineering Costs \$	ROW Year	ROW Costs \$	Construction Year	Construction Costs \$	Total Project Costs \$	Responsible Party	Funding Source	Local Source
TP-9	Internal pedestrian system: Sidewalks Required for New Local Streets (Locations TBD)	Pedestrian: New Facility	2009	80,000	2010	250,000	2012	2,000,000	2,330,000	Private	Private	Private
TP-10	Traffic Calming Devices along Stoney Point Road (7 locations) - <i>Stoney Point is a core team priority</i>	Roadway: Access Mgmt	2010	100,000	2011	50,000	2008	2,000,000	2,150,000	Forsyth County	ARC, FC	Forsyth County
TP-11	Union Hill Road Improvement (includes sidewalks)	Roadway: Capacity	UNDERWAY	75,000	2006	600,000	2007-2009	2,000,000	2,675,000	GDOT, FC	GDOT, FC	Forsyth County
TP-12	McGinnis Ferry Road Improvement (includes sidewalks)	Roadway: Capacity	2010	1,000,000	2006	2,000,000	2007-2009	8,500,000	11,500,000	GDOT, FC	GDOT, FC	Forsyth County
TP-13	New road parallel to Reagan between McFarland and Union Hill (includes sidewalks)	Roadway: Access Mgmt	2010	200,000	2008	1,000,000	2009-2010	2,000,000	3,200,000	Forsyth County	Unidentified / Private	Forsyth County
TP-14	New E-W connector: Shiloh Road East extension, Shiloh to Reagan (includes sidewalks)	Roadway: Access Mgmt	2015	200,000	2016	1,000,000	2012-2015	2,000,000	3,200,000	Forsyth County	Unidentified / Private	Forsyth County
TP-15	New N-S connector: Reagan alternative east of Big Creek, Shiloh NB to Shiloh SB (includes sidewalks)	Roadway: Access Mgmt	2015	200,000	2016	1,000,000	2012-2015	2,000,000	3,200,000	Forsyth County	Unidentified / Private	Forsyth County
TP-16	New E-W connector: Stoney Ridge Drive Improvement (Private Driveways).	Roadway: Access Mgmt	2015	100,000	2016	500,000	2012-2015	1,000,000	1,600,000	Forsyth County	Unidentified / Private	Forsyth County
TP-17	Internal roadway network: New Local Access Streets (Locations TBD)	Roadway: Access Mgmt	2015	80,000	2016	250,000	VARIES	2,250,000	2,580,000	Private	Private	Private
TP-18	GRTA Park and Ride Facility	Transit: New Facility	2006-2007	TBD	2007-2008	TBD	2008-2010	TBD	TBD	GRTA	GRTA	Forsyth County
TP-19	MARTA station (dedicated lanes or rail)	Transit: New Facility	2020-2022	TBD	2022-2023	TBD	2023-2025	TBD	TBD	MARTA	MARTA	Forsyth County
TP-20	McGinnis Ferry GA 400 Interchange	Roadway: New Facility	2010-2012	TBD	2012-2013	TBD	2013-2015	TBD	TBD	GDOT	Federal, State	Forsyth County
TP-21	McFarland Road Improvement (to 6-lanes with raised median)	Roadway: Capacity	2006	400,000	2007-2008	750,000	2008-2010	4,000,000	5,150,000	Forsyth County	Forsyth County, GDOT	Forsyth County
TP-21	Gateway Markers	Streetscape Enhancement	2008-2009	12,500	2010	75,000	2008	250,000	337,500	Forsyth County	Forsyth County	Forsyth County
TOTALS				5,877,500		14,025,000		67,705,000	87,532,500			



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Transportation-Related Initiatives

Project ID	Description/Action	Cost	Year	Responsible Party	Funding Source
TRI-1	Develop Corridor Overlay District to regulate Access Management on Arterials (including McFarland, McGinnis Ferry, Union Hill, Ronald Reagan).	STAFF TIME	2005	Forsyth County	Forsyth County
TRI-2	Investigate the feasibility of a GA 400 Setback Ordinance.	STAFF TIME	2006-2008	Forsyth County	Forsyth County
TRI-3	Perform a GA 400 / McGinnis Ferry Road Interchange Impact Study and coordinate with Fulton County & GDOT.	STAFF TIME	2006-2008	Forsyth County	Forsyth County

Land Use Initiatives

Project ID	Description/Action	Cost	Year	Responsible Party	Funding Source
LUI-1	Create New Zoning Districts				
	Create new zoning districts, as necessary, which will accomplish LCI goals.	TBD	2006	Forsyth County	Forsyth County
LUI-2	Design Guidelines				
	Determine urban design guidelines for subareas within the LCI Study Area and insert into an overlay district.	TBD	2006	Forsyth County	Forsyth County
LUI-3	Adopt New Future Land Use Classifications				
	Through minor amendment to the Comprehensive Plan, according to DCA regulations and County review process, adopt new land use classifications for the LCI Study Area that accomplish LCI goals.	TBD	2006	Forsyth County	Forsyth County

Economic Development Initiatives

Project ID	Description/Action	Cost	Year	Responsible Party	Funding Source
EDI-1	Create Mixed Use Village Destination				
	The village concept is particularly important for long-term sustainability and viability. It helps to provide a reason to stay and re-invest in the community in this time of transience and mobility; opening options to people of every walk of life. The village becomes the amenity and identity that holds value for the community, both financially and emotionally.	TBD	2008	Forsyth County	Forsyth County



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Project ID	Description/Action	Cost	Year	Responsible Party	Funding Source
EDI-2	Conference Center				
	A conference center would be a significant asset to the Study Area, particularly in terms of helping to create a destination location. This conference center location could help anchor the mixed-use village concept. Additionally, it could leverage the Big Creek Watershed Area, as well as the upcoming greenway.	TBD	2008	Forsyth County	Forsyth County
EDI-3	Leverage Lanier Tech				
	The Forsyth campus of Lanier Technical College is located near the northern end of the Study Area, on Majors Road. Its campus is sizable and it is certainly a destination, bringing students from North Fulton, Forsyth, Dawson and Lumpkin counties. The diversity of class offerings ranges from technical education in a variety of occupational disciplines to lifelong learning educational opportunities.	TBD	2006	Forsyth County	Forsyth County
EDI-4	Leverage Northside Hospital				
	Northside Hospital-Forsyth is actively expanding their facilities to meet demand, with approximately \$100 million in improvements planned. That has an immediate economic impact. The hospital does not only serve needs of its patients, it serves the greater community as a major employer, major buyer, and major player	TBD	2006	Forsyth County	Forsyth County
EDI-5	Spearhead Forsyth area economic development campaign				
	Undertake partnership efforts with the City of Cumming and the Cumming/Forsyth Chamber of Commerce to pool resources and maximize exposure of the area as a whole in terms of economic development.	TBD	2006	Forsyth County	Forsyth County
EDI-6	Develop development package of information for Study Area				
	Multiple packages should be developed for different targets, such as relocating and expanding businesses, developers and real estate brokers. This package should have up-to-date information related to competitive advantages, and answer the question, "Why Forsyth?"	TBD	2006	Forsyth County	Forsyth County
EDI-7	Develop objective process and incentives package for quality development				
	Develop an objective system that produces only the quality development you desire, using a mechanism such as a checklist with standards and criteria for approval.	TBD	2007	Forsyth County	Forsyth County
EDI-8	Leverage tourism opportunities				
	Forsyth is in a unique position with access to Lake Lanier and close proximity to the North Georgia Mountains. The Study Area should become a place that people could use as "base camp" to conduct day trips to North Georgia Mountains, and even outlet shopping in Dawsonville.	TBD	2006	Forsyth County	Forsyth County

